



## City of Sonoma Planning Commission **AGENDA**

*Regular Meeting of August 13, 2015 -- 6:30 PM*  
*Community Meeting Room, 177 First Street West*  
*Sonoma, CA 95476*

Meeting Length: No new items will be heard by the Planning Commission after 10:30 PM, unless the Commission, by majority vote, specifically decides to continue reviewing items. If an item is not heard due to the length of the meeting, the Commission will attempt to schedule a special meeting for the following week. If a special meeting is necessary, potential dates will be established at the close of this meeting, and a date set as soon as possible thereafter.

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**CALL TO ORDER** – Chair, Bill Willers

Commissioners: Michael Coleman  
James Cribb  
Robert Felder  
Mark Heneveld  
Chip Roberson  
Ron Wellander  
Robert McDonald (Alternate)

*Be Courteous - **TURN OFF** your cell phones and pagers while the meeting is in session.*

**PLEDGE OF ALLEGIANCE**

**COMMENTS FROM THE PUBLIC:** Presentations by audience members on items not appearing on the agenda.

**MINUTES:** Minutes from the meeting of July 9, 2015.

**CORRESPONDENCE**

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**ITEM #1 – PUBLIC HEARING**

**REQUEST:**

Consideration of a request to allow a metal roof on a second-story residential as a revision to the conditions of approval for an Exception.

**Applicant/Property Owner:**

Darrel and Catherine Jones

**Staff:** David Goodison

**Project Location:**

348 Patten Street

**General Plan Designation:**

Low Density Residential (LR)

**Zoning:**

**Planning Area:** Central-East Area

**Base:** Low Density Residential (R-L)

**Overlay:** Historic (/H)

**RECOMMENDED ACTION:**

Commission discretion.

**CEQA Status:**

Categorically Exempt

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**ITEM #2 – PUBLIC HEARING**

**REQUEST:**

Consideration of a Use Permit to allow an auto body repair shop within an existing auto repair building.

**Applicant/Property Owner:**

G&C Auto Body/Gary and Terrie Heon

**Staff:** Rob Gjestland

**Project Location:**

19285 Sonoma Highway

**General Plan Designation:**

Commercial (C)

**Zoning:**

**Planning Area:**

West Napa/Sonoma Corridor

**Base:** Commercial (C)

**Overlay:** None

**RECOMMENDED ACTION:**

Approve with conditions.

**CEQA Status:**

Categorically Exempt

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**ITEM #3 – PUBLIC HEARING**

**REQUEST:**

Consideration of a Use Permit to allow vocational specialty businesses within a mixed-use building.

**Applicant/Property Owner:**

Extraordinary Ventures of California/Peak Napa Street Associates LLC

**Staff:** Wendy Atkins

**Project Location:**

430 West Napa Street

**General Plan Designation:**

Mixed Use (MU)

**Zoning:**

**Planning Area:**

West Napa/Sonoma Corridor

**Base:** Mixed Use (MX)

**Overlay:** None

**RECOMMENDED ACTION:**

Approve with conditions.

**CEQA Status:**

Categorically Exempt

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**ITEM #4 – PUBLIC HEARING**

**REQUEST:**

Consideration of an Exception to the garage setback requirements to enclose a carport.

**Applicant/Property Owner:**

Richard Konecky

**Staff:** David Goodison

**Project Location:**

753 Third Street East

**General Plan Designation:**

Low Density Residential (LR)

**Zoning:**

**Planning Area:** Central-East Area

**Base:** Low Density Residential (R-L)

**Overlay:** None

**RECOMMENDED ACTION:**

Commission discretion.

**CEQA Status:**

Categorically Exempt

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**ITEM #5 – STUDY SESSION**

**REQUEST:**

Study session on a proposal to construct a mixed-use building with ground floor commercial use and three upstairs condominiums.

**Applicant/Property Owner:**

Kibby Road, LLC

**Staff:** David Goodison

**Project Location:**

19366-19370 Sonoma Highway

**General Plan Designation:**

Mixed Use (MU)

**Zoning:**

**Planning Area:**

West Napa/Sonoma Corridor

**Base:** Mixed Use (MX)

**Overlay:** None

**RECOMMENDED ACTION:**

Provide direction to applicant.

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**ITEM #6 – DISCUSSION**

**REQUEST:**

Consideration of an amendment to the Development Code that would identify “grazing” as a conditionally-allowed use in the “Park” zoning district.

**Staff:** David Goodison

**RECOMMENDED ACTION:**

Forward to City Council, with recommendation for adoption.

**CEQA Status:**

Not Applicable

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**ISSUES UPDATE**

**COMMENTS FROM THE COMMISSION**

**ADJOURNMENT**

I do hereby certify that a copy of the foregoing agenda was posted on the City Hall bulletin board on August 7, 2015.

CRISTINA MORRIS, ADMINISTRATIVE ASSISTANT

**Rights of Appeal:** Any decision of the Planning Commission may be appealed to the City Council. Appeals must be filed with the City Clerk within fifteen (15) calendar days following the Planning Commission’s decision, unless the fifteenth day falls on a weekend or a holiday, in which case the appeal period ends at the close of the next working day at City Hall. Appeals must be made in writing and must clearly state the reason for the appeal. Appeals will be set for hearing before the City Council on the earliest available agenda. A fee is charged for appeals.

*Copies of all staff reports and documents subject to disclosure that relate to any item of business referred to on the agenda are available for public inspection the Monday before each regularly scheduled meeting at City Hall, located at No. 1 The Plaza, Sonoma CA, (707) 938-3681. Any documents subject to disclosure that are provided to all, or a majority of all, of the members of the Planning Commission regarding any item on this agenda after the agenda has been distributed will be made available for inspection at the Administrative Assistant office, No. 1 The Plaza, Sonoma CA during regular business hours.*

*If you challenge the action of the Planning Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described on the agenda, or in written correspondence delivered to the Administrative Assistant, at or prior to the public hearing.*

*In accordance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Clerk (707) 933-2216. Notification 48-hours before the meeting will enable the City to make reasonable arrangements to ensure accessibility to this meeting.*

August 13, 2015  
Agenda Item #1

## **M E M O**

**To:** Planning Commission

**From:** David Goodison, Planning Director

**Re:** Application of Darrel Jones for an amendment to the conditions of approval associated with an exception to the FAR standards approved for a residence at 348 Patten Street

### **Background**

In 2014, Darrel Jones applied for a building permit to remodel and expand an existing single-family residence on the subject property. The residence was a one-story structure having an area of 1,660 square feet, including the attached garage. The project proposed in the building permit submittal included a second floor addition. As a result of a staff error in reviewing the plans, the building permit was issued even though the Floor Area Ratio limit was exceeded. This mistake was discovered in February 2015, when a neighboring property concerned about the mass of the building reviewed the building plans with planning staff. By that time, primary construction was largely complete, with walls, framing, and the roof structure in place. In a follow-up review, it was determined that the FAR limit is exceeded by 458 square feet, although if the elevator is only counted once, the FAR would be reduced by 90 square feet, to 0.40, which is still in excess of the normal limit of 0.35. Because the City cannot issue a certificate of occupancy until the FAR is resolved, Mr. Jones elected to apply for an Exception to the FAR standard as a means of bringing the residence into compliance. This application was reviewed by the Planning Commission at its meeting of July 9, 2015, at which time the Commission voted 6-1 to approve the Exception, subject to conditions. (Note: this decision has been appealed to the City Council and any change in roof design would be contingent upon the outcome of the appeal.)

### **Property and Project Description**

The subject property is a ±6,808 square-foot parcel located on the north side of Patten Street, east of Fourth Street West, developed with a single-family residence. The property is unusual in that it is an interior lot with no street frontage. The applicant has constructed a second-story residential addition in conjunction with a substantial remodel of the first floor. According to the project narrative provided for the FAR Exception request, the remodel/addition is designed in a craftsman style and the residence features sloping roofs and gables on the north and south. As built, the residence has a ridge height of 30 feet, sloping down to 11.5 feet. The front-yard setback (on the west) is 37 feet, while the rear yard setback (on the east) is 23 feet. The side yard setbacks are 12 feet on the north and 7 feet on the south (see attached site plan and elevations.)

### **Request**

In the review of the Exception application, it was noted that although the elevations that had been presented showed a composition shingle roof, the applicant, in his presentation to the

Commission, stated that it was his intent to make use of a standing-seam metal roof. The conditions of approval for the Exception require that the residence be completed in accordance with the submitted building plans. The Planning Commission was not willing to amend the conditions to allow for a metal roof at that time, because no design had been presented. It is the applicant's preference to use a standing-seam metal roof and, therefore, he has applied for amendment to the conditions of approval in order to allow for this change. As detailed in the project narrative, the main arguments made by the applicant in support of the design change are as follows:

- Longer life and greater energy efficiency, leading to improved sustainability.
- Improved compatibility with a planned rain-water harvesting system.

With regard to a specific concern about reflectance raised by a neighboring property owner of the south, Mr. Jones provides a diagram that suggests that the angles of the roof are such that there would never be a direct reflection of light from roof into the neighboring property. In addition, the applicant notes that a low-gloss finish would be used in conjunction with a neutral color. In support of his contention that the roof will be compatible with the design of the residence and with its surroundings, the applicant provides photos of other residences with metal roofs, including some in the Sonoma area and some demonstrating the manufacturer's product.

### **Considerations for Review**

In general, the Planning Commission needs to evaluate request this request in the context of the third finding that it made when it approved the FAR Exception. This finding is as follows:

*Granting the Exception will not be detrimental to the public health, safety, or welfare, or injurious to the property or improvements in the vicinity and in the same zoning district*

When it approved the FAR Exception, the Planning Commission found that the design of the residence as submitted was compatible with its surroundings. The Commission must now determine whether the proposed design change would impair the visual compatibility of the residence to the point where this finding could no longer be made. On a related matter, as discussed above, a neighboring resident to the south previously expressed concern about the design of the subject residence and the prospect of a metal roof (see attached correspondence). The project narrative includes detailed information on the proposed roof type and the proposed color is "W74 Parchment", a neutral grey-brown with a non-reflective finish. With regard to the southern elevation, the applicant notes his intent to install solar panels. Under state law, the installation of solar panels on a single-family residence is exempt from local design review, so this element of the project, if implemented, is not subject to City review, except in terms of compliance with applicable Building Code requirements.

### **Recommendation**

Commission discretion. **Note:** Any decision on this matter will be contingent upon City Council action on the appeal, as the City Council could take a wide range of actions on the project, including requiring changes in the roof design and material selection.

Attachments

1. Conditions of Approval (July 13, 2015)
2. Site Plan
3. Approved Elevations
4. Correspondence
5. Project Narrative

cc: Darrel Jones (via email)

Fred O'Donnell (via email)

Dennis and Joy Donnelly  
567 Fourth Street East  
Sonoma, CA 95476

Zac and Amy Weinberg  
350 Patten Street  
Sonoma, CA 95476

City of Sonoma Planning Commission  
**CONDITIONS OF PROJECT APPROVAL**  
Jones FAR Exception – 348 Patten Street

July 9, 2015

1. The addition shall be constructed in conformance with the project narrative, site plan and elevations.

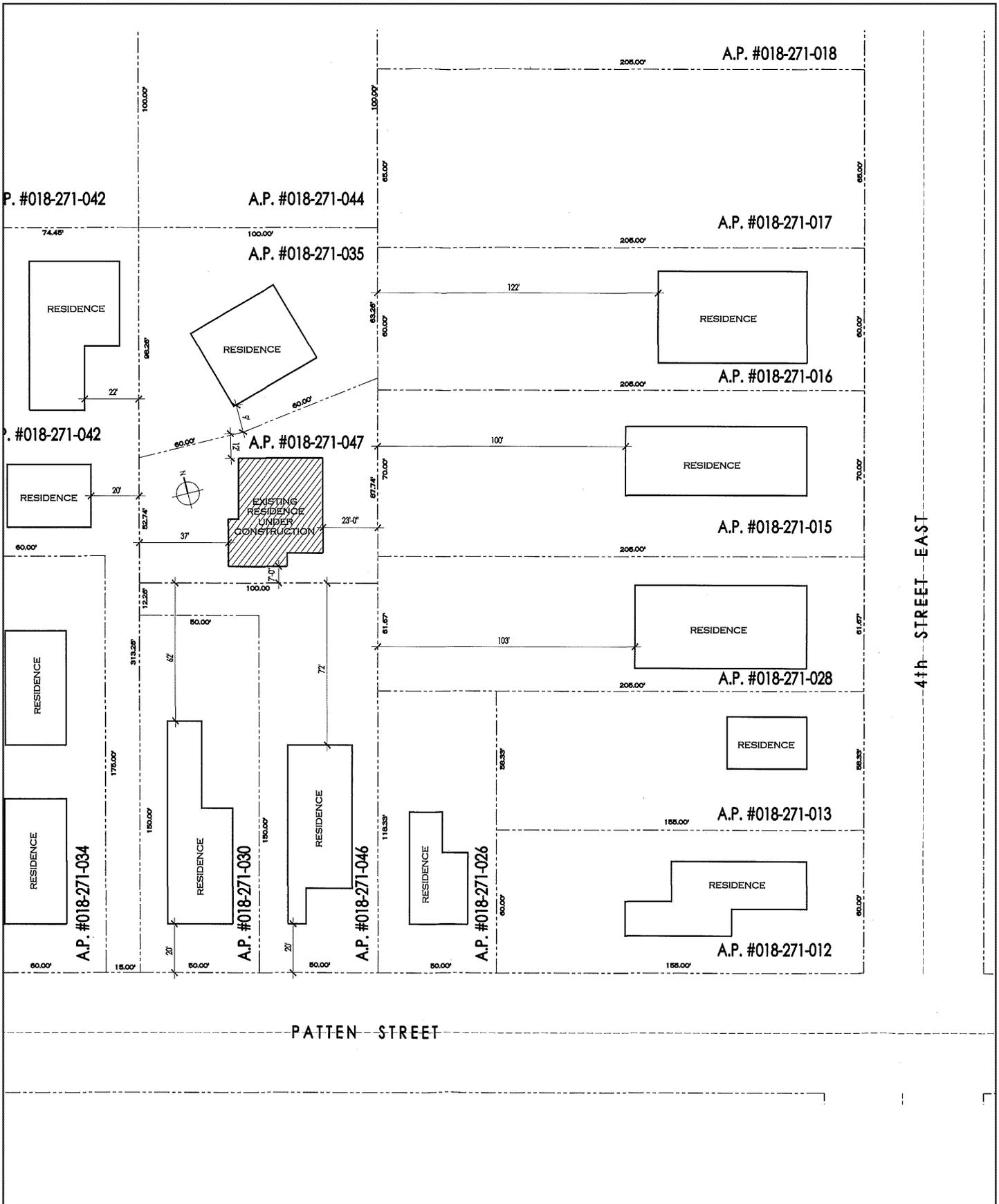
*Enforcement Responsibility: Planning Department; Building Department*

*Timing: Prior to issuance of final occupancy permit.*

2. All Building Department and Fire Department requirements shall be met..

*Enforcement Responsibility: Building Department*

*Timing: Prior to issuance of final occupancy permit.*



**348 Patten Street**  
**Jones Roof Change**

Revisions:  
 A, OCTOBER 27, 2014

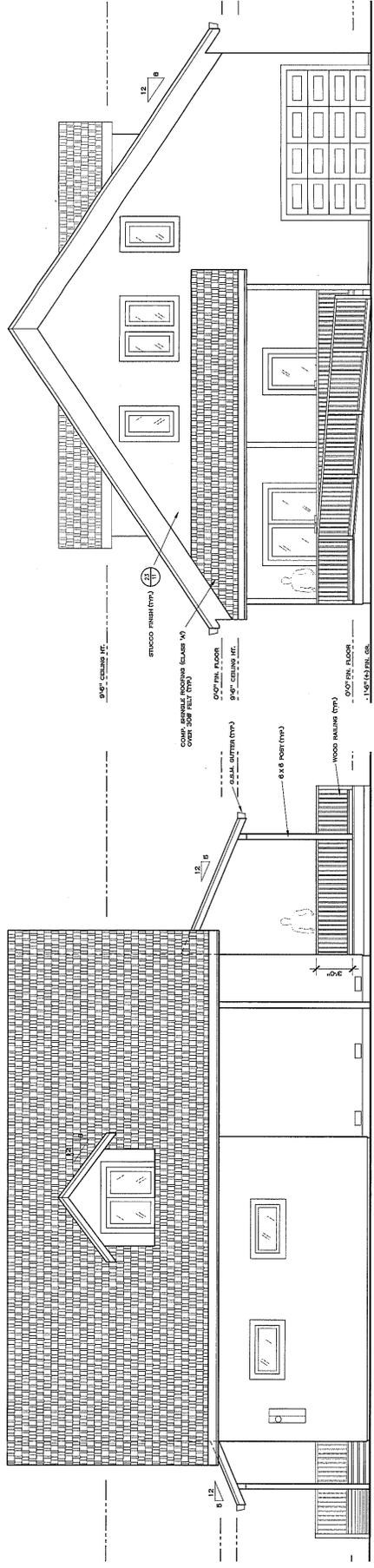
**F I G O**  
**CONSTRUCTION DRAWINGS**  
 PROFESSIONAL  
 REG. NO. 20017-0000  
 707 598-0112 FAX  
 707 598-0112 WORK  
 WWW.FIGONLINE.COM

**JONES RESIDENCE**  
 Addition / Remodel  
 348 Patten Street  
 Sonoma, California

Date: MAY 24  
 Scale: AS NOTED

Proposed  
 Exterior  
 Elevations

**5**

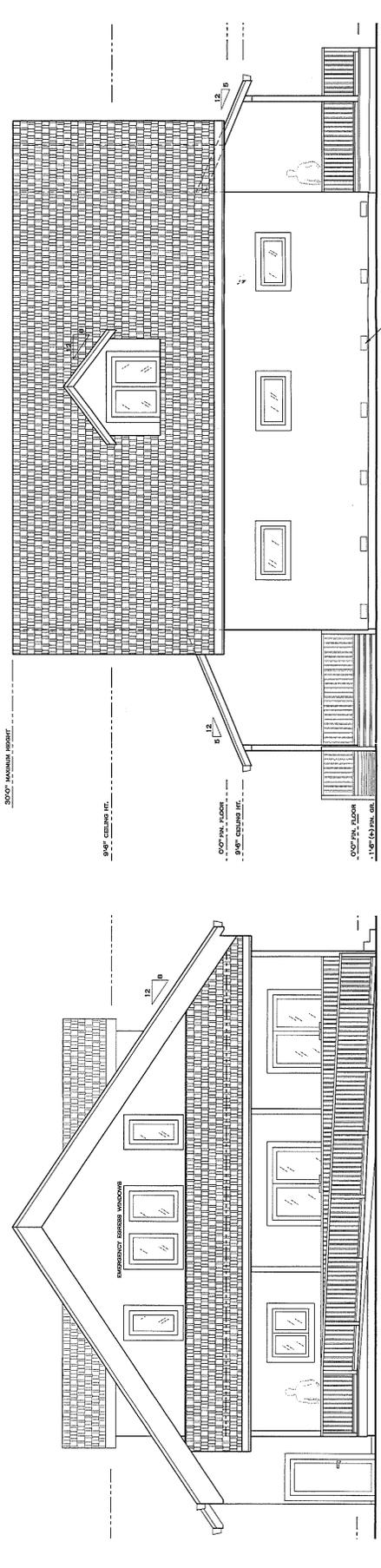


PROPOSED SOUTH ELEVATION

1/4" = 1'-0"

PROPOSED WEST ELEVATION

1/4" = 1'-0"



PROPOSED EAST ELEVATION

1/4" = 1'-0"

PROPOSED NORTH ELEVATION

1/4" = 1'-0"

SEE SHEETS 9 & 10 FOR TYPICAL DETAILS & NOTES

Item # 2  
LATE MAIL

RECEIVED

JUL 08 2015

CITY OF SONOMA

July 7, 2015

Planning Commission  
Sonoma City Hall  
No. 1 The Plaza  
Sonoma, CA 95476

Re: 348 Patten Street

Dear Planning Commission,

My name is Zac Weinberg and my wife Amy and I live at 350 Patten Street. It has been brought to our attention by the city and our neighbor Darrel Jones that his project at 348 Patten Street has resulted in a public hearing. We were under the impression that permits and public comment were completed prior to us purchasing our home in 2014. It is for this reason that we have been hesitant to voice our opinion about the structure that towers over our back yard.

From our perspective, the new structure does not fit in with the neighboring homes and does not fit the lot size. I am not sure how the plans made it through design review, as the structure simply seems too big for the lot and seems to obstruct the view of several neighbors.

We purchased our home under the assumption that a large, two-story, home would not be built directly behind us. The filtered view and privacy that we had is now lost. Please see the attached photos that illustrate the proximity of the home to our property and how it changes the feel of our home. There is a window looking right into our home and back yard. We are concerned that our home value will be impacted by the presence of the large home next door.

During a conversation with Darrel on June 7<sup>th</sup>, I expressed my concern about the impact on my property. I also expressed concern that a roof material be used that will not produce any reflection into my home.

Please note, we truly like the Jones family and wish to not impact our neighborly relationship. If possible, we request this letter remain private. I apologize that this letter is reaching your office so close to the public hearing. That being said, we feel our opinion should be documented since we are next door to the new structure.

Sincerely,

Zac and Amy Weinberg  
[Zac.weinberg@gmail.com](mailto:Zac.weinberg@gmail.com)  
(530) 417-6164

View from back yard of 350 Patten Street prior to construction



View from 350 Patten Street today



Mr. David Goodison, Planning Director

City of Sonoma

#1 The Plaza

Sonoma, California 95476

To whom it may concern:

As a neighbor of Darrel and Catherine Jones on Patten Street, I would like to stand in support of the metal roof they are planning to install on the new structure they are currently building. I feel the metal roof is higher in efficiency and environmentally more beneficial to the long term of the structure. And with the rainwater recovery system in place they would certainly be a front runner. There are many metal roofs in Tahoe and Tahoe Basin and they are very pretty and do not reflect the sun.

Thank you for your careful study in this matter.

Susan Lewis Scarbrough

A handwritten signature in cursive script that reads "Susan Lewis Scarbrough". The signature is written in black ink and is positioned to the right of the printed name.

303 Patten Street

Sonoma, CA 95476

**APPLICATION FOR CHANGE OF ROOFING MATERIALS**  
**348 PATTEN STREET, SONOMA**  
**DARREL AND CATHERINE JONES**  
**JULY 17, 2015**

**SUPPORTING MATERIALS**

- PROJECT NARRATION FOR 348 PATTEN STREET
- 358 PATTEN ROOF SOLAR ELEVATION AND REFLECTANCE ANGLES
- CITY OF SONOMA COOL ROOF REROOFING REQUIREMENTS
- METALMAG MAGAZINE RAINWATER CAPTURE ARTICLE
- COOL METAL ROOFING COALITION INFORMATION
- METAL SALES MFG. SUSTAINABILITY INFORMATION
- METAL SALES COLOR GUIDE AND SOLAR DATA TABLE
- PHOTOS OF EXISTING TWO-STORY METAL ROOF HOUSES
- CITY OF SONOMA RAINWATER CATCHMENT SYSTEMS HANDOUT NO. 36
- CITY OF SONOMA DEFERRED SUBMITTAL AND DESIGN CHANGE APPLICATION FORM
- ELEVATION DRAWINGS WITH ROOF CHANGES FOR 348 PATTEN STREET

JUL 23 2015

## **PROJECT NARRATIVE:**

This is a description of the request to change the roof type for the residential remodel of 348 Patten Street, Sonoma, from asphalt shingle to standing seam metal roofing.

The drawings submitted for permit approval show shingle roofing. The project is designed as an energy-efficient residential structure and the original intent was to install standing seam metal roofing for the superior energy savings and rainwater recovery attributes of this type of roofing system. The reference to a shingle roof was overlooked during the plan review and correction process. The plans were approved and permit issued showing shingle roofing.

In a letter from Zac and Amy Weinberg, 350 Patten Street, dated July 7, 2015, to the Planning Commission, they "expressed concern that a roof material be used that will not produce any reflection into my home." The Planning Commission asked us to bring our proposed change in roofing material before them for their review and consideration. Normally this change would be accomplished with a Deferred Submittal and Design Change Application reviewed and approved by City of Sonoma Planning and Building staff without Planning Commission review. (See City of Sonoma Design Change form).

The attached drawing "ROOF SOLAR ELEVATION AND REFLECTANCE ANGLES" shows that reflection angles from the roof at 348 Patten will be well above the horizon and will not reflect light directly into their home at any time during the year. Additionally, most of the south roof surface will be covered by photovoltaic solar panels, further reducing any exposed metal roof surface. The color chosen, w74 Parchment, is also a low-gloss finish which does not reflect as much light as a standard metal roof finish.

The City of Sonoma's Cool Roof Reroofing Requirements for Existing Low-Rise Residential Buildings requires roofing with minimum solar reflectance of 0.15, thermal emittance of 0.75 or a minimum solar reflective index of 10. The proposed metal roof for 348 Patten exceeds all City of Sonoma requirements, with a solar reflectance of 0.41, thermal emittance of 0.85 and solar reflective index of 45. (See MS Metal Sales Color Guide table and City of Sonoma Cool Roof Handout No. 34).

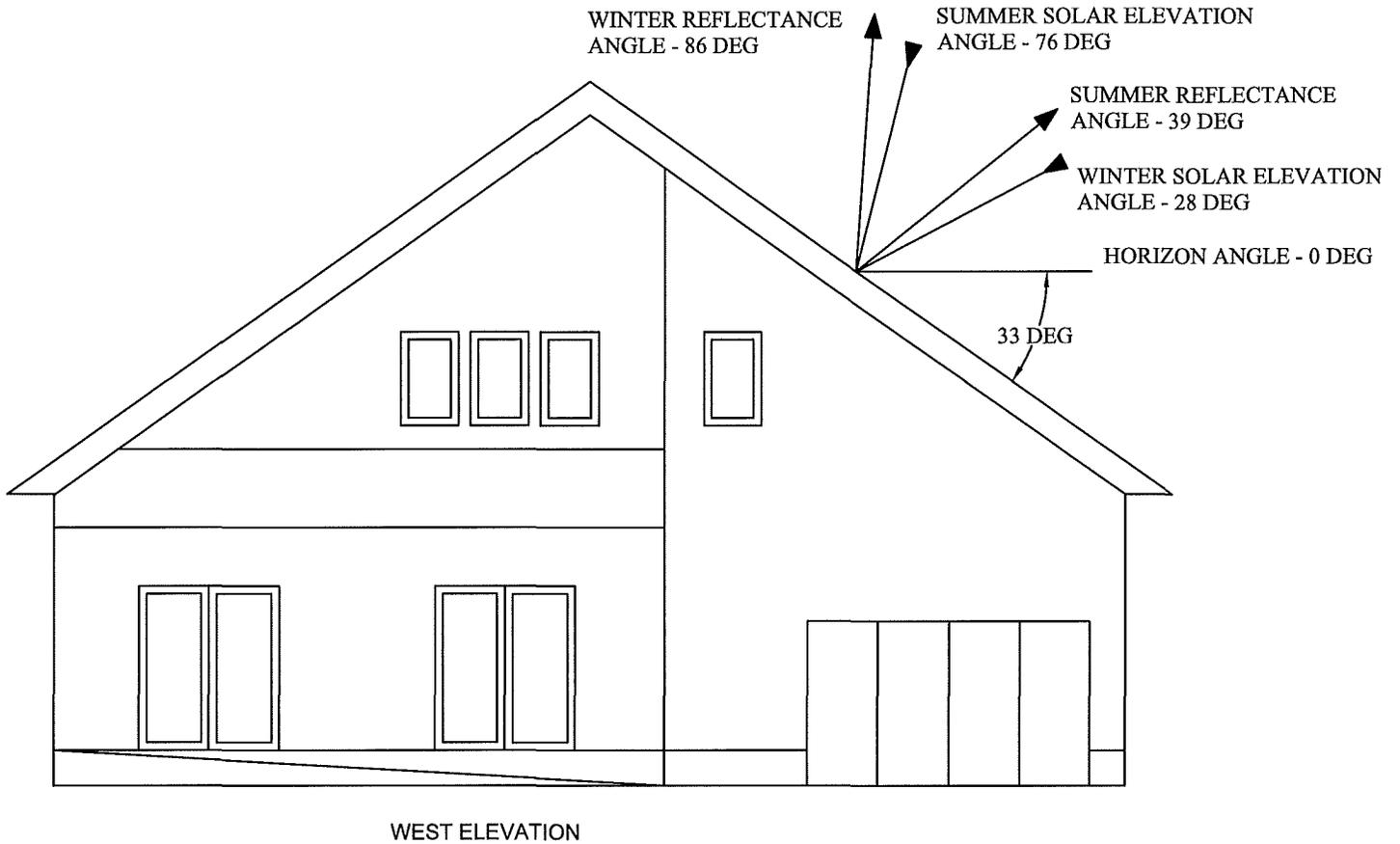
348 Patten is registered as a LEED (Leadership in Energy & Environmental Design) project, and as such, incorporates rainwater recovery as one of its important basic design features. Rainwater recovery from metal roofs does not suffer from the petroleum byproducts contamination associated with asphalt shingles. While the recovered rainwater is used for irrigation in this case, it will be watering vegetables for human consumption. The attached article from MetalMag, February 2010, by Jim Schneider, Field Tech, details the advantages of metal roofing for rainwater recovery and highlights the pathogen and toxin contamination associated with asphalt shingle and other permeable roofing materials. (See "Metal Roofs Provide an Ideal Platform for Rainwater Capture" and City of Sonoma Rainwater Catchment Systems Handout No. 36).

Finally, as a LEED registered project to be certified to LEED standards, sustainability is key to the design, construction and eventual reconstruction of this house. As detailed in the attached information from MS

Metal Sales and the Cool Metal Roofing Coalition, metal roofs offer excellent sustainability with the raw materials, production and eventual recycling of the materials used in its manufacture. With energy savings of up to 40% over other roofing methods, the recyclability, long life cycle, integration into Net-Zero building and Green strategies, and building re-use make metal roofing the preferred method for environmentally compatible building systems. (See "MS Metal Sales: Sustainability/Our Products" and "Cool Metal Roofing").

There are numerous homes in Sonoma with metal roofing, including a PHIUS (Passive House Institute of the United States) certified Passive House shown in the included photographs. Other photos included with this application show a number of building styles, including Craftsman style homes, using metal roofing. We ask the Planning Commission to approve our application to change the roofing methods from asphalt shingle to metal roofing for 348 Patten Street in keeping with the design goals of the project.

Darrel and Catherine Jones  
358 Patten Street  
Sonoma, CA



SOLAR ANGLES ARE FOR SUMMER AND WINTER SOLSTICE AT 12:00 PM (NOON) LOCAL TIME. ANGLES ARE FROM THE SUNANGLE CALCULATOR USING LATITUDE 38.28 N AND LONGITUDE 122.47 W FOR SONOMA, CA. ANGLES ARE SHOWN FOR THE SOUTH FACING ROOF OF 348 PATTEN STREET. DISTANCE FROM THE EDGE OF THE ROOF TO THE RESIDENCE AT 350 PATTEN STREET IS APPROXIMATLY 90 FEET.

348 PATTEN STREET, SONOMA  
 ROOF SOLAR ELEVATION AND REFLECTANCE ANGLES

DARREL AND CATHERINE JONES  
 358 PATTEN STREET, SONOMA

DATE: 7/18/2015

REV: 1

**SA-1**



# City of Sonoma

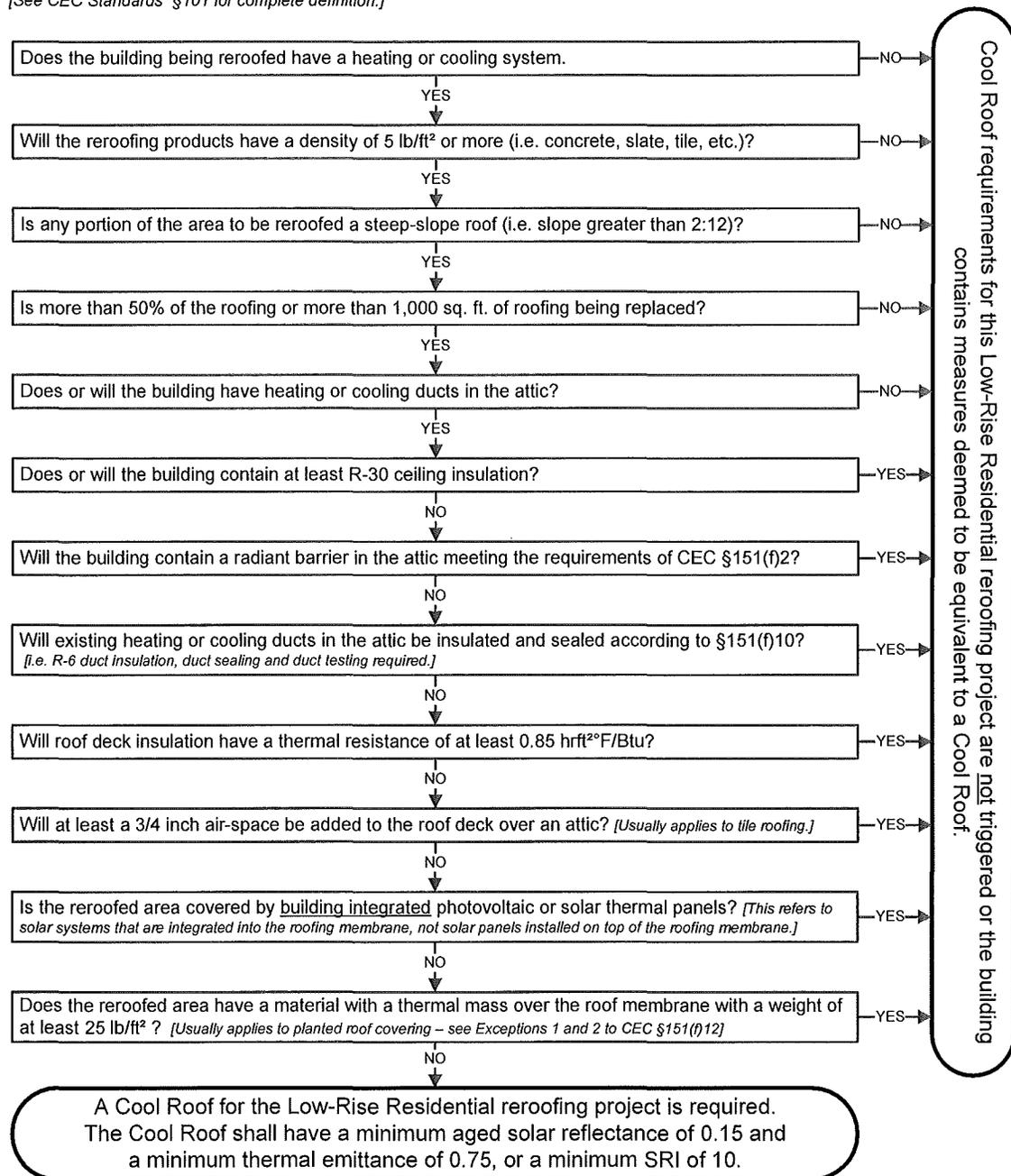
## Building Department Informational Handout

### Cool Roof Reroofing Requirements for Existing Low-Rise Residential Buildings

Handout No: 34  
Published: September 11, 2009  
Effective: January 1, 2010

**Note:** This chart applies to existing Low-Rise Residential Buildings in Climate Zone 2 (Sonoma). As an alternative to these requirements, an applicant may submit energy calculations to show that the building complies with the California Energy Code Performance Approach. A completed CF-1R-ALT form must be submitted with all low-rise residential reroofing permit applications. The effective date of these requirements is January 1, 2010.

A **LOW-RISE RESIDENTIAL BUILDING** is typically a one- or two-family dwelling or a multi-family residential building with three stories or less or a building such as a private garage, carport, barn, greenhouse, shed or agricultural building located on a residential site. Hotels, motels or transient boarding houses are not considered a Low-Rise Residential Building. [See CEC Standards §101 for complete definition.]



**Use Only for Low-Rise Residential Reroofing in Climate Zone 2 (Sonoma)**

Project Address:

Reroofing Area:

**Part 1 – Determination of Requirements** (check all items that apply)

A Low-Rise Residential building to be reroofed is **exempt** from Cool Roof requirements if **any** of the following items apply:

- The building being reroofed has no heating or cooling systems. [CEC Table 100-A]
- The building being reroofed is entirely a low slope roof (slope less than or equal to 2:12 pitch). [CEC §152(b)H.ii]
- The proposed reroofing material has a density of less than 5 lb/ft<sup>2</sup> (i.e. composition shingle, wood shingle or shake, metal roofing).
- Less than or equal to 50% of the roofing and less than or equal to 1,000 sq. ft. of roofing is being replaced. [CEC §152(b)H]
- The re-roofed area will include building integrated photovoltaic or solar thermal panels. [CEC §151(f)12B]
- The re-roofed area is covered with a material with a thermal mass over the roof membrane with a weight of at least 25 lb/ft<sup>2</sup>.

[NOTE: Buildings exempt from Cool Roof requirements may utilize the City's Contractor Self-Certification Program]

Cool Roof requirements are **required** for a Low-Rise Residential building if it has:

- A steep slope roof (greater than 2:12 pitch) with reroofing products that have a density of 5 lb/ft<sup>2</sup> or more (i.e. concrete, slate, tile, etc.) The Cool Roof shall have a minimum aged solar reflectance of 0.15 and a minimum thermal emittance of 0.75, or a minimum SRI of 10. [Please complete Parts 1, 2 and 3 of this form.]

**In lieu of installing a Cool Roof**, any of the following "equivalent" items may be used to meet the Cool Roof requirements:

- The building has no heating or cooling ducts in the attic, or
- The building contains or will contain at least R-30 ceiling insulation, or
- The building has or will be provided with a compliant radiant barrier in the attic, or
- Existing heating or cooling ducts in the attic have been sealed, tested and insulated with R-6 insulation per CEC 151(f)10 [NOTE: test results must be submitted and approved by Building Department], or
- Roof deck insulation has or will have a thermal resistance of at least 0.85 hr-ft<sup>2</sup>-°F/Btu, or
- At least a 3/4 inch air-space exists or will be added to the roof deck over an attic. [CEC §152(b)H]

[NOTE: Buildings that must install Cool Roofs or equivalent may not utilize the City's Contractor Self-Certification Program]

**Part 2 - Reroofing Products**

Note: If no CRRC-1 label is available, this compliance method cannot be used, use the Performance Approach to show compliance.

CRRC Product ID Number <sup>1</sup>	Product Type <sup>2</sup>	Aged Solar Reflectance <sup>3,4</sup> <i>0.15 minimum</i>	Thermal Emittance <i>0.75 minimum</i>	SRI <sup>5</sup> <i>10 minimum</i>
		<input type="checkbox"/> <sup>4</sup>		
		<input type="checkbox"/> <sup>4</sup>		
		<input type="checkbox"/> <sup>4</sup>		

1. The CRRC Product ID Number can be obtained from the Cool Roof Rating Council's Rated Product Directory at [www.coolroofs.org/products/search.php](http://www.coolroofs.org/products/search.php).
2. Indicate the type of product is being used for the roof top, i.e. single-ply roof, asphalt roof, metal roof, etc.
3. If the Aged Reflectance is not available in the Cool Roof Rating Council's Rated Product Directory then use the Initial Reflectance value from the same directory and use the equation  $(0.2 + 0.7(\rho_{initial} - 0.2))$  to obtain a calculated aged value. Where  $\rho$  is the Initial Solar Reflectance.
4. Check box if the Aged Reflectance is a calculated value using the equation above.
5. The SRI value needs to be calculated from a spreadsheet at <http://www.energy.ca.gov/title24/>

**Part 3 - Declaration**

**Documentation Author's Declaration Statement**

- I certify that this Certificate of Compliance documentation is accurate and complete.

Name:		Signature:	
Company:		Date:	
Address:		EA # CEPE #	
City/State/Zip:		Phone:	

MetalMag — February 2010  
Field Tech  
Jim Schneider

## **METAL ROOFS PROVIDE AN IDEAL PLATFORM FOR RAINWATER CAPTURE**

It's no secret that the fortunes of entire civilizations and the very survival of life on our planet are tied closely to rainfall. Getting the right amount in the right places at the right time is just as important to us today as it was thousands of years ago.

Though most take the water in their homes for granted, in many parts of the world and even in parts of the U.S., factors like drought, pollution and increased population have created a scarcity of this vital resource. As traditional supplies dry up, more municipalities and businesses, as well as homeowners, are beginning to look to the sky. The concept of rainwater catchment is not new, but the applications are growing in scope and gaining in popularity. Systems to collect rainwater for things like irrigation, toilet flushing and drinking are being made available to homeowners and businesses.

**OPTIONS ABOUND:** The advantages to capturing rainwater are many. It's an excellent water conservation technique, whether applied in a drought-stricken area or a location where water is more plentiful. Rainwater is free and, by collecting it, a building can significantly reduce its reliance on municipal water supplies. Also, collecting the rainfall that falls on a building or property is an efficient way to manage stormwater and prevent runoff, which aids in slowing soil erosion and minimizing pollution of rivers, lakes and streams.

Most rainwater capture systems are fairly simple in design and operation, but can vary in levels of complexity. According to a report by the Austin, Texas-based Texas Water Development Board titled "The Texas Guide on Rainwater Harvesting," all such systems share a number of basic components: a catchment surface (such as a roof) that the rainwater falls from; a system of gutters and downspouts; components that remove dust and debris from the water before it goes to the tank (such as leaf screens and first flush diverters); one or more storage tanks; a delivery system (typically either gravity fed or pumped); and a treatment/ purification system for potable end-use systems.

There are many factors to consider when approaching a project that intends to incorporate this technique. "For most projects, it'll work in any of several ways," explains Billy Kniffen, water resource specialist for the Texas Agrilife Extension Service, which is part of Texas A&M University, College Station. "We look at the stormwater management aspect and whether the system is intended for passive collection, trying to keep the water there on the site, or trying to capture water off the roof and into an aboveground or below-ground storage tank. Is the water intended for the commode, for irrigation or for whole-house use? There are lots of parameters to examine."

**CATCHMENT SURFACE:** No matter what the final use, the catchment surface is the foundation upon which the entire system rests. It is important to minimize any dirt, debris or chemicals the water may pick up so there is less to filter later in the process. For this reason, metal roofs are ideal for many projects that install rainwater capture systems.

"We like metal roofs to begin with because they have a slicker surface than composite or other systems, so it's not going to collect as much dust and debris," says Kniffen. "It washes off much more quickly.

It's the most effective, quickest and most reliable solution. With other types of systems, you have to worry about grit, bird droppings and other debris that is hard to wash off non-metal roofs." Metal does hold several advantages compared to other roofing materials that might be used as catchment surfaces. Composite or asphalt roofs can be used for irrigation applications, but due to the fact that the materials hold and harbor so many pathogens and can leach toxins, they are not preferred for potable water systems. The same holds true for wood shingle, tar and gravel roofs.

Clay and concrete tile roofs can be used for potable end use, but because of the porous nature of the material, there is a greater danger of bacterial growth in the pores. In addition, the pores naturally absorb water and porous catchment surfaces like these can experience as much as a 10 percent water loss resulting from inefficient flow and evaporation.

"Whether it's colored material or Galvalume, a slick metal roof is preferred in every area I work in," Kniffen says. The slick surface washes off quickly and easily and a diversion of the first water that runs off the roof is minimized when compared to other surfaces. "We consider composition roofing acceptable with some caution. If it's been impregnated with lead, copper or zinc as an inhibitor to prevent mold, then there is a concern. There have been some arguments over copper roofs because copper is toxic and low pH rainwater will erode it a little bit," Kniffen warns. "If you do use such a system on a copper roof, make sure you test your water if you're going to be drinking it.

Most of the research I've seen has said copper is acceptable, but every homeowner and business needs to test that water and make sure the copper level isn't too high."

**GOING DOWN:** From the catchment surface, the next place the water goes is down. The trick is getting it to the tank in an efficient and clean way.

The first thing to consider is the design of the roof itself. Many roofs, particularly on homes, can be made up of many peaks and valleys. Noting how the rainwater flows before placing the gutters is very important.

Factors like steep roof slopes, long distances from ridge to eave, lack of maintenance and too few downspouts can cause gutters to overrun. Depending on the roof conditions, it may be

necessary to modify the size and configuration of the gutters to properly accommodate the rainwater.

Leaf screens and filters typically are used to remove debris that gathers on the catchment surface. Although they are critical in a catchment system in removing large debris from the rainwater, prices and quality vary between products and some necessitate more maintenance than others and may be prone to clogging, requiring them to be maintained and watched.

Another technique to remove debris from the water before it gets to the tank is the use of first-flush diverters. Basically, the device diverts the initial flow of water away from the storage tank. This first flow will clean the roof of things like dust, insects, pesticides and bird droppings. With this initial quantity of water flushed away from the tank, the water that follows it will have had the benefit of falling on a clean roof.

There are a number of devices that can perform the task. One is a PVC standpipe, which fills with water, backs up, then allows water to flow into the main collection piping.

Another type is standpipe with a ball valve, which is similar in design but includes a ball that floats up and creates a seal that traps the first-flush water. The standard rule is that 10 to 20 gallons for every 1,000 square feet of roof must be diverted as part of the first flush.

Finally, the water makes its way to the storage tank. The most expensive component of the rainwater catchment system, storage tanks are available in myriad types, depending on the location, budget and intended end use of the system. They can be above or below ground and can be made from concrete, fiberglass, metal, wood or polypropylene. The tanks must be opaque to inhibit algae growth.

**ON THE RISE** Although the economic case may not yet be strong on its own—comparing 1,000 gallons of captured rainwater versus 1,000 gallons of municipal water still indicates a rather long buyback period on a catchment system—more states and municipalities are encouraging the systems through incentives and even mandates.

“In Texas, we’ve tried to pass and probably will pass a law that every new state building of 10,000 square feet or more has to have a collection system to capture the rainwater off that building,” Kniffen predicts. “In some cities there is a stormwater fee if you allow water to run off your site. In Santa Fe, any new house of 2,500 square feet or more must have a collection system on it before they will give you a building permit.” Mandates or no, owners and designers are looking to the rainwater capture as a conservation and sustainable measure. “More people in the industry are looking to do rainwater catchment, partly because of the trend toward green building,” Kniffen says. “More LEED points are being given to rainwater capture than were previously. Many different states are adopting procedures to encourage it and most are trying to figure out how to reduce stormwater runoff and improve water quality. In the Southwest it’s vital to their survival.



## **Sustainability / Our Products**

Metal Sales is committed to advancing sustainable design and building objectives, including architectural strategies toward energy efficiency, LEED certification and Net-Zero.

For 50 years, Metal Sales™ has provided quality metal roof and wall products favored for their durability and performance in all weather conditions including windstorms, rainstorms, snow, hail and ice.

Our portfolio of durable products is ideal for use in high performance building, whether new or retrofit.

Metal Sales products are materially efficient, promote resource optimization and divert waste from landfill.

### **Recyclability**

The vast majority of Metal Sales products are made from steel, the world's #1 most recycled material. The steel we use contains a minimum of 30% recycled content and are 100% recyclable at the end of their long lifespan.

Also, compared to asphalt shingles, Metal Sales residential roofing products are far better for the environment. A very small percentage of asphalt shingles are ever recycled; most are landfilled. Plus, asphalt shingles are petroleum-based products, which further tax our world's reliance on fossil fuels.

### **Long Life Cycle**

All Metal Sales products enjoy very long life cycles compared to alternative products. Products that don't require replacement (or require less frequent replacement) during the lifespan of a building are generally preferred from the perspective of sustainability.

#### **Energy Conservation**

Metal Sales products optimize energy and minimize construction waste. All Metal Sales color coating systems are ENERGY STAR® listed, and many are certified by the Cool Roof Rating Council. In both cases, these accreditations signify that Metal Sales products help to reduce the

solar heat load on homes and buildings and thereby help reduce energy expenditures for cooling.

### **Integration with Net-Zero Building and Green Strategies**

Metal Sales continues to develop methods that enable renewable energy technology to be more easily integrated into our products and systems. Examples include the easy integration of polycrystalline and thin-film laminate photovoltaics, micro wind turbines, solar heating and rainwater harvesting systems into our metal roofing systems.

### **Living Building Challenge Declare Label**

The Living Building Challenge (LBC) certification program, administered by the International Living Future Institute, takes a broad view of sustainability and embraces the philosophy of a restorative future by looking at a building's performance over time. In fact, certification is not granted until the building has been occupied and its performance documented for one year.

The Institute's Declare Label is an ingredients-based eco-label around the Red List of "chemicals of concern" that have human health and toxicity impacts. Declare aims to provide transparency and open communication by allowing manufacturers to voluntarily share their product sources, materials and manufacturing locations.

Metal Sales™ is the first metal panel manufacturer to be included in the rigorous and exclusive Declare™ program. Metal Sales has fully disclosed all of the ingredients in the Acrylic Coated Galvalume® roof and wall panels through Declare, and they are designated as being Red List Free on the Declare Label.

Metal Sales has fully disclosed all of the ingredients in the Acrylic Coated Galvalume® roof and wall panels through Declare, and they are designated as being Red List Free on the Declare Label.

For more information, please visit [www.declareproducts.com](http://www.declareproducts.com)

### **Building Re-Use**

Metal Sales offers products and systems designed to make remodeling and retrofitting of homes and buildings a more viable option than demolishing and re-building them. Metal Sales Retro-Master™ re-roofing system exemplifies one such product.



# Cool Metal Roofing

**B**uilders and architects are changing the way they select building materials and design for energy performance. The roof can have the greatest impact on the energy use of a building. Coatings and finishes available today qualify metal as a recognized cool roof product.

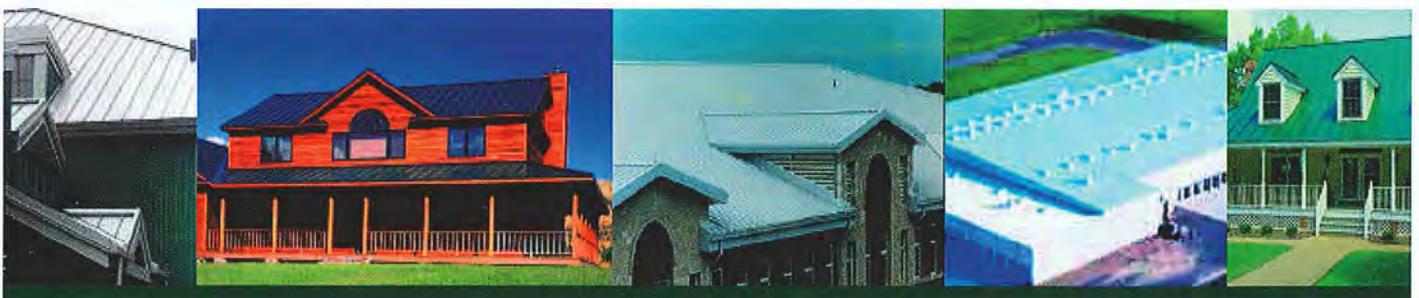


*Metal Roofing--  
cool and sustainable*



**Buildings consume one-third of all energy and two-thirds of all electricity generated. Cool roofs can help reduce energy consumption by lowering cooling loads. Reflective roofs directly save up to 40% in heating and cooling energy costs, as reported by Oak Ridge National Laboratory. Highly emissive roofs can lower urban air temperatures, thereby benefiting the environment by reducing smog formation.**

**Metal roofing has many attractive features. Its architectural appeal, variety of profiles, textures and color, flexibility, and durability make it popular for residential and commercial projects, both in low-slope and steep-slope applications. Metal roofing and its finishes are inert, safe materials that don't pose a health risk. Metal roofing is tested for wind, fire, and hail resistance, and listed with various building codes and entities. Its non-combustibility can reduce the spread of fire in and among buildings.**





*Metal roofing is the choice for a cool roof.  
For more information about its energy efficiency, visit  
[www.coolmetalroofing.org](http://www.coolmetalroofing.org)*

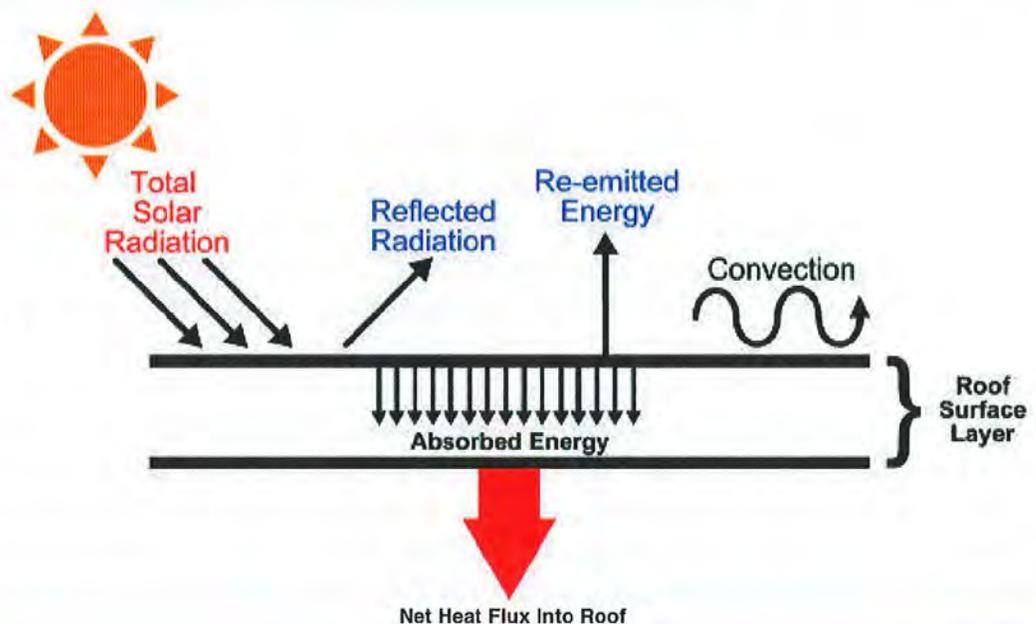
## ENERGY EFFICIENCY

A building's cooling and heating costs can be effectively reduced by insulation under the roof surface. Adding increasing amounts of insulation is not always the best way to save energy. As part of total system design, a cool metal roof can be an economical method for better energy efficiency.

Cool metal roofing is available unpainted, with oven-baked paint finishes, or with granular-coated surfaces. This family of roofing can achieve solar reflectance of over 70 percent. Reflected solar energy allows the roof surface to remain cooler, which means less heat is transferred into the building.

The infrared emittance of a roof is a measure of absorbed solar radiation that is re-emitted from the roof surface to the sky. Emittance of metal roofing varies with the surface finish. Emittance of painted or granular-coated metal roofing can be as high as 90 percent.

Where annual cooling loads dominate, a highly reflective and highly emissive painted or granular-coated metal roof is optimal for reducing energy consumption. Where annual heating loads dominate, an unpainted metal roof is more desirable because of its low infrared emittance.



**The Florida Solar Energy Center found that metal roofing...**

**"...saves the most energy as a result of its high reflectance and superior ability to cool quickly at night."**

**Metal roofing was reported to save a Florida homeowner about 23% annually in cooling costs, compared to a dark gray asphalt shingle roof.**



*Metal roofing is the choice for a cool roof.  
For more information about its energy efficiency, visit  
[www.coolmetalroofing.org](http://www.coolmetalroofing.org)*

## MITIGATING THE URBAN HEAT ISLAND EFFECT

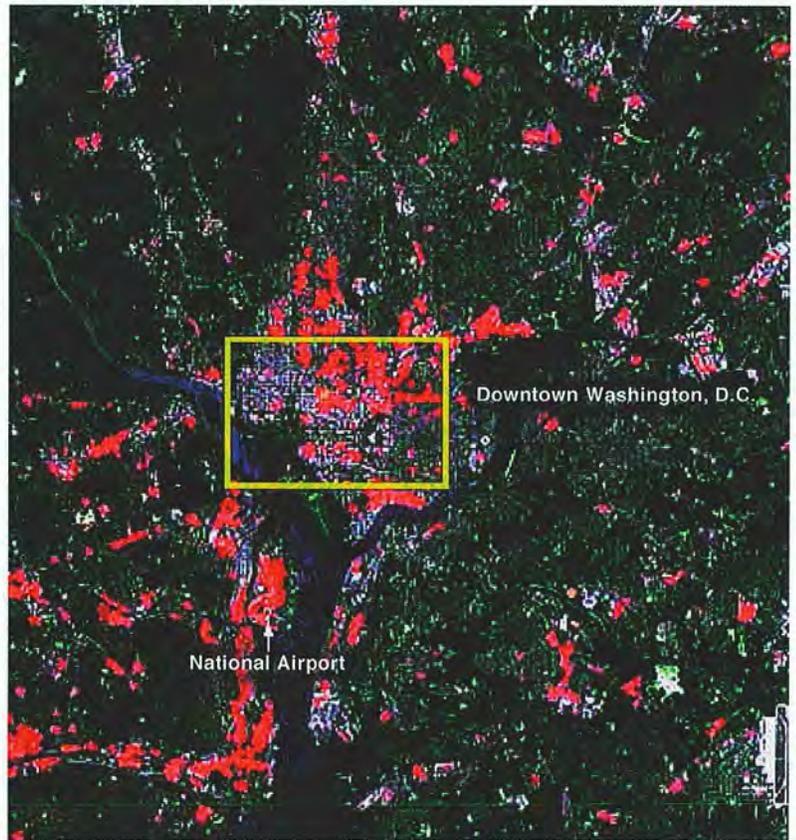
About half of the North American population lives in urban areas. The prevalent use of energy-absorbing, dark building materials and dark pavements, coupled with a lack of vegetation, creates a microclimate where ambient temperatures are higher than in the surrounding areas. For some cities, this urban heat island effect can increase the local temperature as much as 12° F (7° C).

Cool metal roofing is one way to mitigate the urban heat island effect. Roofs with higher reflectance have lower surface temperatures, which help reduce ambient air temperatures. This improves air quality since less smog is formed. Also, air pollution associated with burning fossil fuels at utility plants is reduced because of less peak load demand.

Oak Ridge National Laboratory research has shown that metal roofing retains solar reflectance over time better than other roofing products because it resists the growth of organic matter and sheds dirt more readily than other materials.

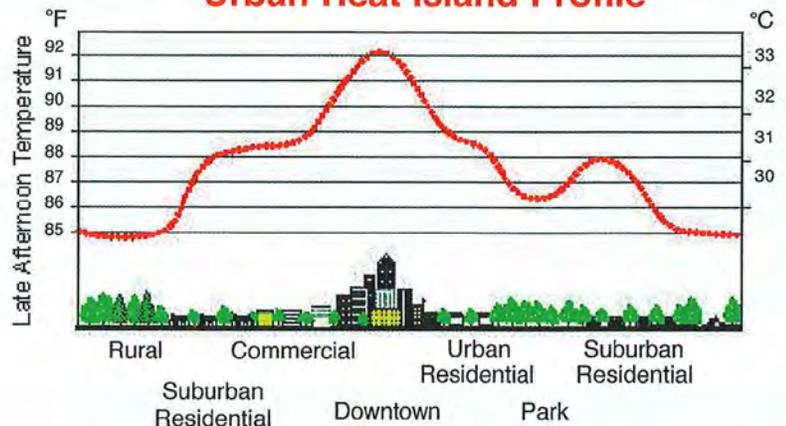
Metal roofs are included on the U.S. EPA's Energy Star Roof Products Program. Metal roofs are also represented on the California Energy Commission's Cool Roof Savings program listing.

**Systems for World Surveillance**



Hottest surfaces colored in red

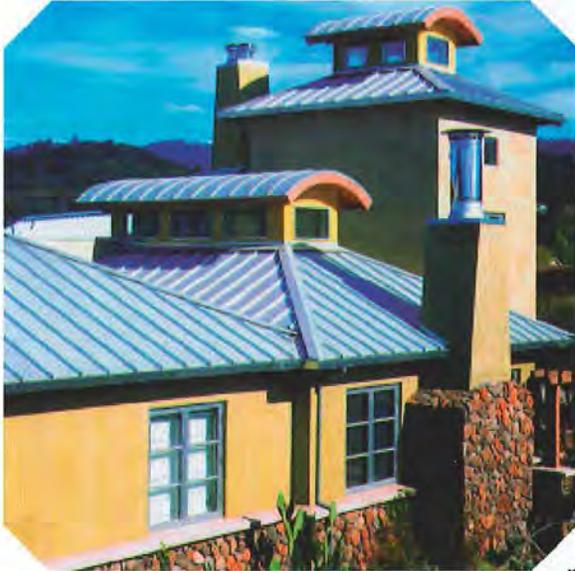
### Urban Heat Island Profile





*Metal roofing is the choice for a cool roof.  
For more information about its energy efficiency, visit  
[www.coolmetalroofing.org](http://www.coolmetalroofing.org)*

## SUSTAINABILITY



In addition to being energy efficient, metal roofing is recognized as a sustainable building material for several other environmental reasons. And, as a "green" building product, metal roofing is rightfully becoming more popular in the architectural community.

**Durability** - Metal roofing is known for its resistance to weather, including wind, hail, ice, and snow. It is less affected by hot-cold and wet-dry cycles that destroy other materials. Commercial metal roofs have been in service 30 years or more. Painted roofs are credibly warranted up to 30 years.

**Recycled Content** - Metal roofs typically have a minimum of 25% recycled content. This level of recycled content allows metal roofing to be routinely included on listings for "green" and recycled content products. This is especially important since the U.S. Green Building Council program for certification of green buildings, Leadership in Energy and Environmental Design (LEED)<sup>™</sup>, awards either one or two points for the weighted total recycled content of all materials in a project. Metal roofing is a solid contributor toward one LEED point and may help gain a second one.

**Recyclability** - Metal roofing is also 100% recyclable when ultimately removed as part of building renovation or demolition. Other roofing materials are routinely removed and disposed of by the ton in landfill, but metal roofing can be recycled in its entirety. Metals are exceptional building materials that can credibly claim both recycled content and recyclability by recognized definitions.

**Low Weight** - Depending on the specific product chosen, the weight of metal roofing is one-third to as little as one-eighth that of conventional roofing shingles. This lower weight produces less static and dynamic loading on the structure and thus can allow for metal roofing to be installed over old roofing material without requiring demolition removal.

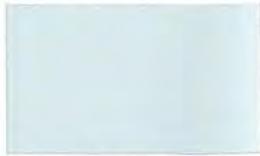


**Developed by the Cool Metal Roofing Coalition**

680 Andersen Drive, Pittsburgh PA 15220-2700  
For more information, visit: [www.coolmetalroofing.org](http://www.coolmetalroofing.org)

FLAGSHIP03 08/03

## PVDF (Kynar 500®) Paint System



Snowdrift White (W81)



Linen White (81)



Sandstone (W51)



Parchment (W74)



Taupe (74)



Khaki (88)



Medium Bronze (H4)



Weathered Copper (W50)



Mansard Brown (133)



Dark Bronze (50)



Ash Grey (25)



Old Town Grey (W25)



Old Zinc Grey (W29)



Slate Grey (W38)



Matte Black (106)



Aged Copper (65)



Patina Green (W58)



Hemlock Green (M7)



Classic Green (66)



Felt Green (W66)



Patriot Red (73)



Terra Cotta (W72)



Colonial Red (W75)



Brandywine (P8)



River Teal (59)



Metallic Silver (K7)¹



Champagne Metallic (168)¹



Mistique Plus (W31)¹



Copper Penny (W92)¹



Antique Patina (M1)¹



Tahoe Blue (W71)



Ocean Blue (35)



Regal Blue (W35)



Galvalume® (41)  
Non-painted Finish  
25 Year Warranty



All Colors Meet or Exceed Steep Slope ENERGY STAR® Requirements

¹ An up-charge may apply

Visit [www.metalsales.us.com](http://www.metalsales.us.com) for valuable tools and resources.

### 45 Year Paint Warranty

All colors carry a 45 year limited paint warranty  
Color selections are close representations but are limited by  
printing and viewing conditions. Actual samples are available by request.

# 24 GAUGE

PVDF (Kynar 500®) Color	Solar Reflectance ASTM C 1549	Thermal Emittance ASTM C 1371	Solar Reflectance Index ASTM E 1980	Low Gloss	Metallic Finish	ENERGY STAR® Steep Slope*	ENERGY STAR® Low Slope*	CRRC Steep Slope*	CRRC Low Slope*	LEED Steep Slope*	LEED Low Slope*
Aged Copper (65)	0.32	0.85	32			•		•		•	
Antique Patina (M1)	0.38	0.85	40		•	•		•		•	
Ash Grey (25)	0.38	0.86	41			•		•		•	
Brandywine (P8)	0.26	0.85	24			•		•			
Champagne Metallic (168)	0.47	0.85	53		•	•		•		•	
Classic Green (66)	0.32	0.86	33			•		•		•	
Colonial Red (W75)	0.35	0.86	37	•		•		•		•	
Copper Penny (W92)	0.45	0.85	50		•	•		•		•	
Dark Bronze (50)	0.30	0.86	30			•		•		•	
Felt Green (W66)	0.31	0.84	31	•		•		•		•	
Galvalume® (41)	0.67	0.14	56		•	•				•	
Hemlock Green (M7)	0.36	0.85	38	•		•		•		•	
Khaki (88)	0.35	0.87	37			•		•		•	
Linen White (81)	0.73	0.86	89			•	•	•	•	•	•
Mansard Brown (133)	0.30	0.87	31			•		•		•	
Matte Black (106)	0.27	0.86	26			•		•			
Medium Bronze (H4)	0.30	0.87	31			•		•		•	
Metallic Silver (K7)	0.60	0.77	68		•	•		•		•	
Mistique Plus (W31)	0.34	0.82	34		•	•		•		•	
Ocean Blue (35)	0.29	0.86	29			•		•		•	
Old Town Grey (W25)	0.40	0.85	43	•		•		•		•	
Old Zinc Grey (W29)	0.42	0.85	46	•		•		•		•	
Parchment (W74)	0.41	0.86	45	•		•		•		•	
Patina Green (W58)	0.46	0.85	51	•		•		•		•	
Patriot Red (73)	0.46	0.86	52			•		•		•	
Regal Blue (W35)	0.27	0.86	26	•		•		•		•	
River Teal (59)	0.29	0.86	29			•		•		•	
Sandstone (W51)	0.54	0.86	63			•		•		•	
Slate Grey (W38)	0.30	0.85	30	•		•		•		•	
Snowdrift White (W81)	0.65	0.85	78	•		•	•	•		•	•
Tahoe Blue (W71)	0.30	0.86	30	•		•		•		•	
Taupe (74)	0.29	0.84	28			•		•			
Terra Cotta (W72)	0.39	0.85	42	•		•		•		•	
Weathered Copper (W50)	0.32	0.84	32	•		•		•		•	

\*LOW SLOPE: Surface with a slope of 2:12 or less • STEEP SLOPE: Surface with a slope greater than 2:12

9-12 1318

## Metal Sales Branch Locations

Anchorage, AK: 866.640.7663  
 Bay City, MI: 888.777.7640  
 Deer Lake, PA: 800.544.2577  
 Denver, CO: 800.289.7663  
 Detroit Lakes, MN: 888.594.1394

Fontana, CA: 800.782.7953  
 Fort Smith, AR: 877.452.3915  
 Independence, MO: 800.747.0012  
 Jacksonville, FL: 800.394.4419  
 Jefferson, OH: 800.321.5833

Mocksville, NC: 800.228.6119  
 Nashville, TN: 800.251.8508  
 Rock Island, IL: 800.747.1206  
 Rogers, MN: 800.328.9316  
 Seattle, WA: 800.431.3470

[www.metalsales.us.com](http://www.metalsales.us.com)

Sellersburg, IN: 800.999.7777  
 Spokane, WA: 800.572.6565  
 Temple, TX: 800.543.4415  
 Woodland, CA: 800.759.6019

# EXAMPLES OF HOMES WITH METAL ROOFS WITHIN 2 MILE RADIUS



Metal Roof w/Solar. 760 3rd St. East.  
Within .2 mi

The first Certified Passive House™ in California and the first Certified Passive House™ retrofit in the United States. Builder: PassivWorks.



Metal Roof. 248 France St. Within .2 mi



Metal Roof. 167 4th St. East. Within .5 mi



Metal Roof. 167 4th St. East. Within .6 mi



Metal Roof. 242 Wilking Way. Within .6 mi



Light Colored Metal Roof. 242 Wilking Way.  
Within .6 mi

# EXAMPLES OF HOMES WITH METAL ROOFS WITHIN 2 MILE RADIUS



Metal Roof, Solar panels. West Spain St.  
Within 1.2 mi. Builder: Blu Homes



Metal Roof. Castle Road. Within 1.9 mi



Metal Roof. Castle Road. Within 1.9 mi



Metal Roof. Castle Road. Within 1.9 mi



Metal Roof. 19307 7th St. East. Within 1 mi

Builder: Angeli & Bertlow.

# EXAMPLES OF HOMES WITH METAL ROOFS FROM METAL SALES MANUFACTURING CORPORATION

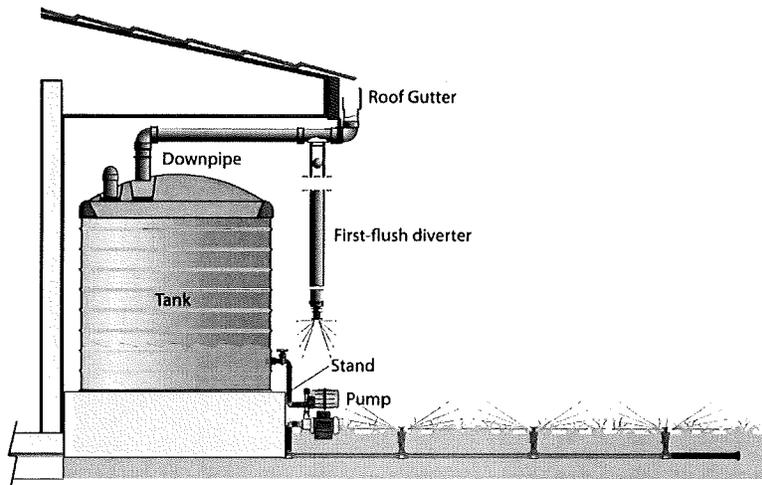




### Rainwater Catchment Systems

#### Rainwater Catchment

Rainwater catchment is the capture, retainage storage and diversion of rainwater flowing off of an impervious surface for subsequent use for landscaping irrigation and other onsite use.



Collection of rainwater is usually from rooftops, which is then stored in rainwater storage tanks. Stored water can be used for non-potable purposes such as irrigating landscaping, washing cars or possibly even flushing toilets. Rainwater catchment systems can range from a simple barrel at the bottom of a downspout to multiple large underground tanks with pumps and controls.

#### Rainwater Catchment System Limitations

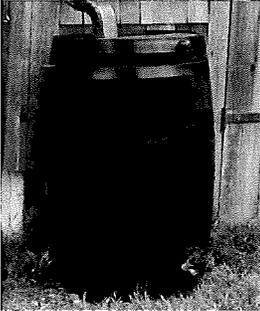
Most rainwater catchment systems are used exclusively for landscaping irrigation purposes. A building permit is usually required for the installation of large storage tanks, distribution systems, and/or pumps and backflow prevention devices associated with a rainwater catchment system. A building permit is always required if the rainwater storage and distribution system is proposed for use inside of a building. Currently, the State of California has not adopted rainwater catchment standards for use within buildings. Section 601.1 of the California Plumbing Code requires that plumbing fixtures including toilets, urinals washing machines and floor drains, be connected to an “adequate supply of potable running water” unless the City Building Official determines that it is not necessary for safety or sanitation reasons. Given that there are cities within the United States and other countries that have safely allowed the use of rainwater for fixtures such as toilets, urinals, washing machines and trap primers in floor drains, City Building Department staff will review and determine the acceptability of such requests on a case-by-case basis when designed by a licensed California Mechanical Engineer that specializes in plumbing or rainwater catchment systems. The installation of a rainwater system within a building requires separately isolated and identifiable piping systems for both the non-potable rainwater system and the potable City water system to prevent contamination between them.



# Requirements for Rainwater Catchment and Storage Systems

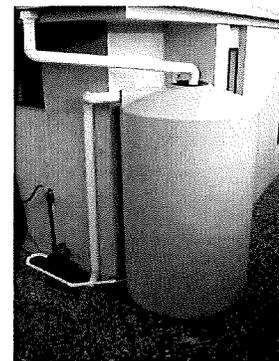
**Zoning.** Rainwater storage barrels are containers with a volume of 80 gallons or less. Larger storage tanks are considered structures for the purposes of determining City zoning and setback requirements. The following enumerates some of the City’s zoning requirements:

## Setbacks for Residential Uses

- A  rainwater storage barrel with a volume of 80 gallons or less may be placed below downspouts around a building without considering front, side or rear yard building setback requirements. *[Planning Department Interpretation]*
  - Rainwater storage tanks that do not exceed 8 feet in height above finished grade and do not exceed 120 square feet in area may be placed immediately adjacent to a side or rear property line. *[Sonoma Municipal Code Section 19.50.080.C.2.b.]*
- Rainwater storage tanks not exceeding 9 feet in height above grade that are separated from other buildings on the property by a 6-foot-wide or more open yard may be placed as close as 5 feet to a side or rear property line. *[Sonoma Municipal Code Section 19.50.080.C.5. and 19.50.080.C.2.a.]*
- Above-ground storage tanks are prohibited in required front and street-side setbacks, and in designated creek setback areas. *[Sonoma Municipal Code Section 19.50.080.C.3.]*
- Above-ground rainwater storage tanks not meeting the above requirements must comply with the building setback requirements set forth in the Article III of the City’s Development Code for the zoning district in which it is located. *[Sonoma Municipal Code Section 19.40.110.A.1]*

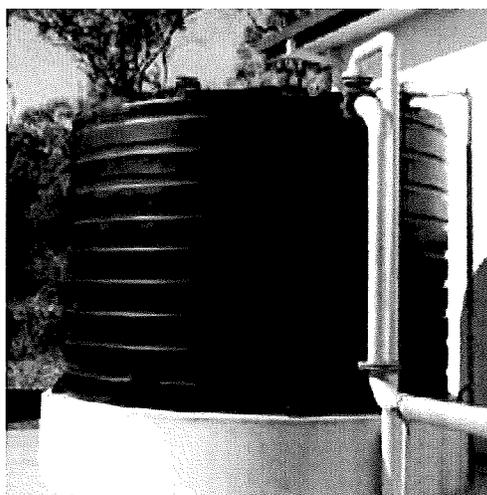
## Setback and Design Review Requirements for Nonresidential Uses

- A rainwater storage barrel with a volume of 80 gallons or less may be placed below downspouts around a building without considering front, side or rear yard building setback requirements. *[Planning Department Interpretation]*
- Rainwater storage tanks not meeting the above requirements must comply with the building setback requirements set forth in the Article III of the City’s Development Code for the zoning district in which it is located. *[Sonoma Municipal Code Section 19.40.110.A.1]*
- Above-ground rainwater storage tanks for nonresidential uses are prohibited in required front and street-side setbacks, and in designated creek setback areas. *[Sonoma Municipal Code Section 19.40.110.A.3, 19.40.110.E. and 19.40.020.D.2 ]*
- Design Review Approval is required for above-ground rainwater storage tanks for which a building permit is required. *[Sonoma Municipal Code Section 19.54.080.B.2]*
- Design Review Approval is required for rainwater storage tanks located in public view. *[Sonoma Municipal Code Section 19.54.080.B.2]* EXCEPTION: Design Review Approval will not be required for single rainwater storage barrels with a volume of 80 gallons or less and placed below downspouts around a building. *[Planning Department Interpretation]*



Permits. A City building permit is required for rainwater catchment and storage systems under any of the following circumstances:

- If the property is connected to the City’s water system and the total combined stored water capacity for the rainwater system exceeds 360 gallons. *[See CPC 1702.2 exception (1). Installation and inspection of a cross-connection control device near the City water meter is required in accordance with Sonoma Municipal Code Section 13.20.030 and City Standard Plan #213.]*
- If the storage tank is not supported directly by the ground or concrete slab or is supported by a raised platform or other structure. *[2013 California Plumbing Code sections 1702.9.5]*
- If the size of a water storage tank exceeds 5,000 gallons or the ratio of height to width of the tank exceeds 2:1. *[2013 Administrative Provisions - Section 106]*
- If electrical pumps, electrical valves or electrical controllers are installed, unless they are cord- and plug-connected or operate at less than 25 volts and not capable of supplying more than 50 watts. *[2013 Administrative Provisions - Section 106]*
- If any portion of the rainwater catchment system is proposed to be used or located inside of a building or for supplying toilets, urinals, trap primers or washing machines. *[2013 California Plumbing Code sections 601.1 and 1702.2.1]*
- If the rainwater storage tank will be installed below or partially below grade. *[2013 California Plumbing Code sections 1702.9.5.4]*



Other Considerations for Rainwater Catchment Systems.

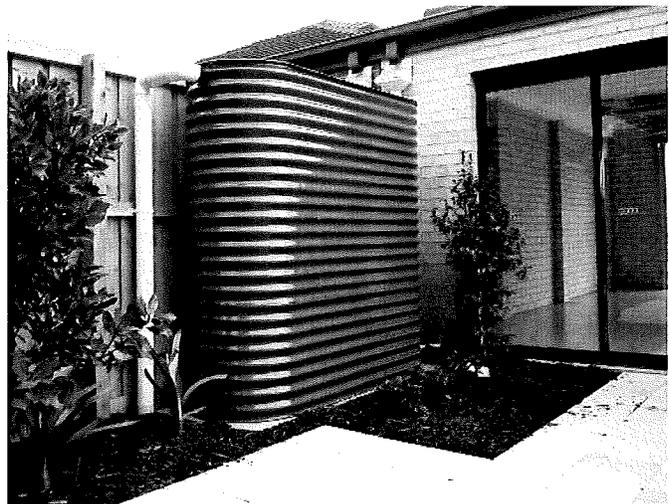
- Most requirements for rainwater catchment systems can be found in Chapter 17 of the California Plumbing Code. The code addresses specific design and safety considerations related to the proposed system including, but not limited to the following:
  - Plan Submission
  - Connections to Potable or Reclaimed (Recycled)
  - Cross-Connection
  - Water Systems
  - System and Pipe Sizing
  - Water Supply, Distribution and Drainage Materials
  - Catchment System Components Color and Marking

- Minimum Water Quality
- Requirements for Rainwater Storage Tanks
- Requirements for Pumps, Roof Drains, Water Quality Devices, Filters,
- Signs
- Inspection and Testing.

- Untreated rainwater may contain contaminants and is considered non-potable water. Non-potable water piping must be exposed to view and must be marked every 20 feet with a yellow identification band and labeled “CAUTION: NON-POTABLE WATER, DO NOT DRINK.” Discharge outlets must be exposed to view and must be labeled with the international symbol for non-potable water and the words “CAUTION: NON-POTABLE WATER, DO NOT DRINK.” *[2013 California Plumbing Code sections 601.2.1.(4), 1702.8 and 1702.9]*



- Rainwater catchment systems may not be directly connected to other potable water sources, such as the City-supplied domestic water system or a well serving the home or business. *[2013 California Plumbing Code - 1702.4]*
- For above-ground water storage tanks exceeding 5,000 gallons or if the ratio of height to width of a tank exceeds 2:1, an engineered pad base and adequate anchorage system must be provided. *[2013 California Building Code Chapter 16 and Section 1604.1]*
- Rainwater storage tanks installed below grade must be adequately tied down and anchored to prevent lifting caused by groundwater.
- Rainwater that is part of a rainwater catchment system must be filtered and treated to meet the minimum filtering and water quality requirements set forth in Table 1702.9.4 of the California Plumbing Code.
- Rainwater storage tanks must be provided with pressure relief mechanisms (vents) and tank overflows to prevent tank pressure buildup and to provide an overflow route should the tanks fill to capacity. Rainwater tanks shall be permanently marked with the capacity and the language: "NONPOTABLE RAINWATER." *[2013 California Plumbing Code - 1702.9.5.8]*
- The rainwater catchment system should be designed as an integrated solution incorporating collection, contaminant removal, pumping, control and distribution. Rainwater tanks should be well sealed, maintainable and be provided with first-flush devices and filters to keep out leaves and other contaminants. Inlet and overflow screens should be provided to prevent access of mosquitoes and other insects and vermin. Provisions should be made for periodically draining and cleaning the rainwater storage tanks of siltation and other contaminants.



- It is important that a rainwater catchment system be designed to prevent growth of algae and other organisms in the system. Good aeration and circulation of water, no sunlight on water and keeping leaves and organic matter in the water to a minimum will help to reduce the growth of algae.
- The rainwater catchment system should be properly sized. Variables such as available capture area, storage availability, spring-time average rainfall and usage requirements must be considered to properly design a system.
- For automated irrigation and distribution systems, consideration should be given to providing makeup water to the tank from sources other than rainwater (i.e. City or well water).

## Rainwater Catchment Resources

- American Rainwater Catchment Systems Association:  
<http://www.arcса.org/>

For further questions, please contact the City of Sonoma Building Department at 707-938-3681.



**City of Sonoma**  
Building Department

#1 The Plaza  
Sonoma, CA 95476

Phone: (707) 938-3681  
Fax: (707) 938-8775

**Deferred Submittal & Design Change Application**

Application Date

Existing Permit Number

Please complete all applicable portions of the application below. (Please print)

Project Street Address		Suite #	
Project or Existing Business Name		APN #	
Applicant (responsible for payment of review)	Phone	E-Mail Address	
Billing Address	City	State	Zip

**NOTE:** This application form is used ONLY for design changes or deferred submittals that do not increase or add to the existing scope of work covered under the existing permit stated above. Changes in the design that add or increase the scope or valuation of work covered under said permit will require a new (separate) permit application. When approved plans for the permit stated above were prepared by a licensed design professional, all proposed design changes for those portions of the plans must be designed and prepared by a licensed design professional.

Description of Deferred Submittal or Design Changes Being Submitted:

I am requesting that the Building Department review and approve design changes to the permit stated above. I agree to save, indemnify, and hold harmless the City of Sonoma and its employees against liabilities, judgments, costs and expenses which may accrue against the City or its employees in consequence of the design changes requested and further agree to pay all required fees in connection with the additional plan review and inspection therewith.

x \_\_\_\_\_  
**Applicant Signature** (Person responsible for payment of review) **Date**

FOR CITY USE ONLY  
**ROUTING AND REVIEW APPROVALS**

Routed To	Approved By	Date	Rate	Review Hours	Review Fee
<input type="checkbox"/> Building	_____	_____	\$97/hr	_____	PLCKB
<input type="checkbox"/> Planning	_____	_____	\$100/hr	_____	PLCKP
<input type="checkbox"/> Fire	_____	_____	\$120/hr	_____	PLCKF
<input type="checkbox"/> Public Works	_____	_____	\$89/Hr	_____	PLCKE

Total Design Change Review Fee: \$ \_\_\_\_\_

Less Initial Design Change Review Deposit (\$48.50 min.): (\$ \_\_\_\_\_)(PLCKB)

**Balance Due:** \$ \_\_\_\_\_

Revisions:  
 A OCTOBER 27, 2014

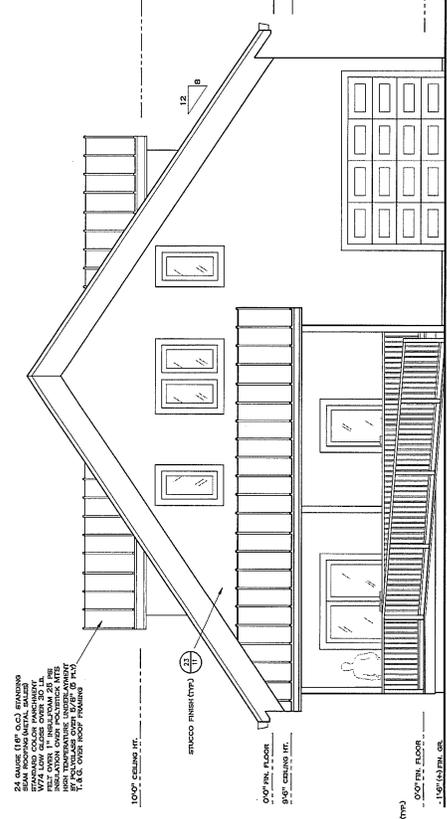
**F I O**  
**G O**  
**CONSTRUCTION**  
 825 BROADWAY  
 BOSTON, MA 02126  
 787-886-0133  
 WWW.FIORIGONSTRUCTION.COM

**JONES RESIDENCE**  
 Addition / Remodel  
 348 Patten Street  
 Sonoma, California

Date: 10/27/14  
 Scale: AS NOTED

Proposed  
 Exterior  
 Elevations

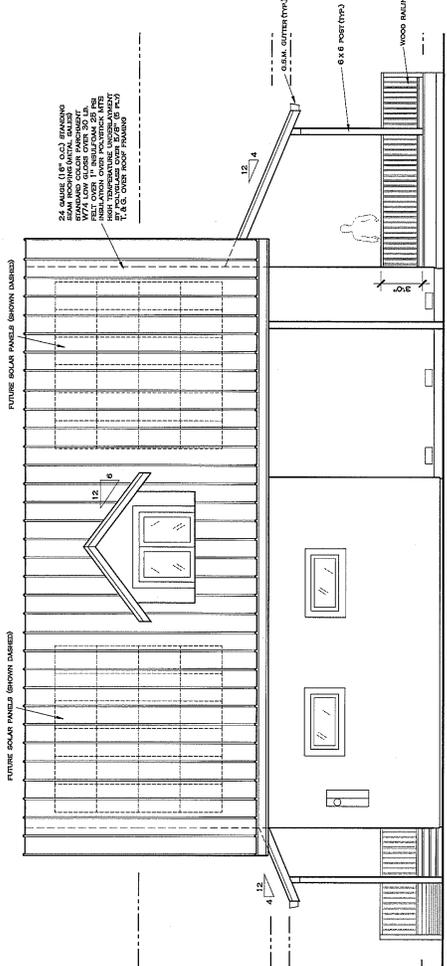
5



PROPOSED SOUTH ELEVATION

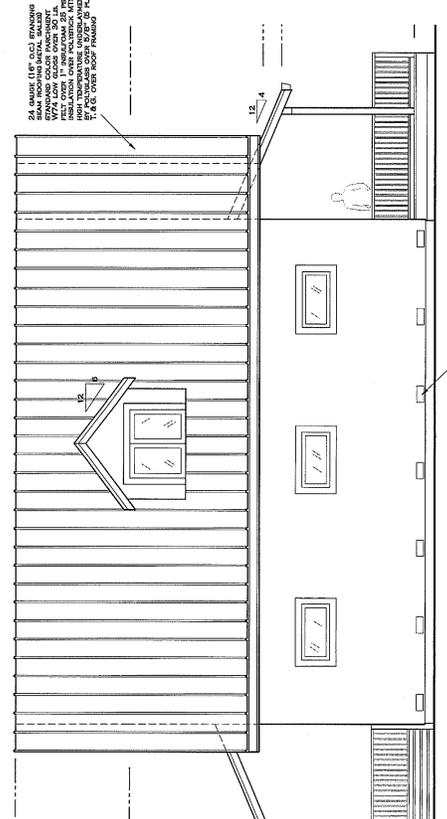
1/8" = 1'-0"

**request for roofing material change**  
 JULY 2015



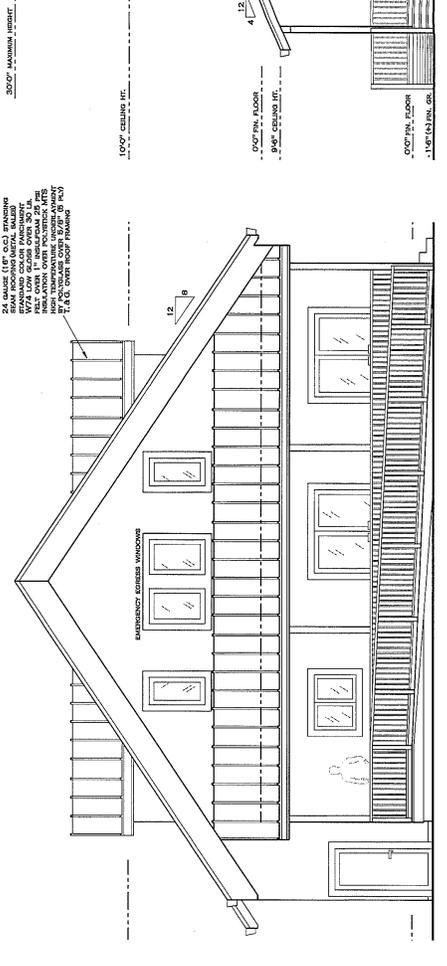
PROPOSED WEST ELEVATION

1/8" = 1'-0"



PROPOSED NORTH ELEVATION

1/8" = 1'-0"



PROPOSED EAST ELEVATION

1/8" = 1'-0"

SEE SHEETS 9 & 10 FOR TYPICAL DETAILS & NOTES

**Agenda Item Title:** Application for a Use Permit to allow an auto body repair shop within an existing auto repair building.

**Applicant/Owner:** G&C Auto Body/Gary and Terrie Heon

**Site Address/Location:** 19285 Sonoma Highway

**Staff Contact:** Rob Gjestland, Senior Planner  
Staff Report Prepared: 08/07/15

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**PROJECT SUMMARY**

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**Description:** Application of G&C Auto Body for a Use Permit to allow an auto body repair shop within an existing auto repair building at 19285 Sonoma Highway.

**General Plan Designation:**

Commercial (C)

**Zoning:**

**Base:** Commercial (CO)

**Overlay:** None

**Planning Area:**

West Napa Street/Sonoma Highway Corridor

**Site**

**Characteristics:**

The subject property is a 13,500-square foot parcel located on the west side of Sonoma Highway. The property is currently developed with a 6-bay car repair building (±4,500 square feet in area) that shares circulation and a common driveway from Ramon Street with the adjoining parcel to the east.

**Surrounding**

**Land Use/Zoning:**

**North:** Gas station with carwash/Commercial

**South:** Restaurant and apartment building site/Commercial

**East:** Carwash and quick lube business/Commercial

**West:** Rancho de Sonoma Mobile home park/Mobile Home Park

**Environmental**

**Review:**

Categorical Exemption

Approved/Certified

Negative Declaration

No Action Required

Environmental Impact Report

Action Required

Not Applicable

**Staff**

**Recommendation:**

Approve, subject to conditions.

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## ***PROJECT ANALYSIS***

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### **DETAILED PROJECT DESCRIPTION**

G & C Auto Body is requesting a Use Permit to operate an auto body repair shop within an existing auto repair building off Sonoma Highway. The building has a floor area of ±4,500 square feet, constructed in 1991 to accommodate a muffler shop (since that time it has also accommodated a smog shop and most recently McLea's Tire & Automotive Center). G & C would provide auto body collision repair services within the building, Monday through Friday from 8a.m. to 6p.m. (some work may also occur on Saturdays). It is anticipated that the business would initially operate with three employees, potentially increasing to five employees. Relatively minor alterations would be needed to support the use, including provision of a paint spray booth within one of the bays. Other proposed modifications include remodeling the office, repainting the building exterior, and installing new entry doors, as well as an awning and signage. Parking for the business would be accommodated on the south side of the property where eight spaces currently exist, plus two spaces near the office entrance on the north. Additional parking would also be available within the building, which has sufficient depth (±45 feet) to accommodate two cars in tandem within the bays. Further details can be found in the attached project narrative and drawings.

### **GENERAL PLAN CONSISTENCY ( Not Applicable to this Project)**

The property is designated Commercial by the General Plan. The Commercial land use designation is intended to provide areas for retail, hotel, service, medical, and office development, in association with apartments and mixed-use developments and necessary public improvements. The auto body business would occupy an existing auto repair building and does not raise any issues of inconsistency with the *City of Sonoma 2020 General Plan*. That being said, the proposal must be evaluated in terms of continued compatibility with adjacent residential uses and parking adequacy (refer to "Discussion of Project Issues" below).

### **DEVELOPMENT CODE CONSISTENCY ( Not Applicable to this Project)**

*Use:* The property is located within a Commercial (C) zoning district, which is applied to areas appropriate for a range of commercial land uses including retail, tourist, office, and mixed uses. Auto body repair facilities are allowed in the Commercial zone, subject to review and approval of a Use Permit by the Planning Commission. Staff would note that the vehicle services that have historically operated on the property (originally approved by the Planning Commission in 1988) are considered minor repair facilities under the Development Code, whereas an auto body shop is categorized as a major repair facility, which is why a Use Permit review has been required by the City.

*Development Standards:* The business would occupy an existing auto repair building. As a result, the project does not raise any issues in terms of compliance with building setback, FAR, lot coverage or height standards.

*On-Site Parking:* Eleven striped parking spaces would continue to be provided on the property under the proposal. While this amount of parking is less than required under the City's current parking standards (four spaces per vehicle bay plus one space per each two employees), it is consistent with the 1988 approvals for the site/muffler shop and the parking requirements in place at that time. Accordingly, the property is legally non-conforming with respect to parking. Staff would note that the use would not increase employee levels (3-5 proposed versus up to 6 employees for the original muffler shop), and the change in type of auto repair service does not have a different parking requirement. As a result, the proposal does not trigger or require any additional parking under the Development Code. That said, it is

important that the business can function adequately within existing parking levels as a practical matter (refer to “Discussion of Project Issues” below).

*Design Review:* The narrative indicates that the building exterior would be repainted along with the provision of a new entry doors, awning and business signage. As normally required, these exterior alterations would be subject to review and approval by the Design Review & Historic Preservation Commission.

**CONSISTENCY WITH OTHER CITY ORDINANCES/POLICIES** ( Not Applicable to this Project)

**ENVIRONMENTAL REVIEW** ( Not Applicable to this Project)

Pursuant to Section of 15301 of the State CEQA Guidelines, the operation, permitting, leasing, or minor alteration of existing private structures or facilities, involve negligible or no expansion of use are categorically exempt from the provisions of CEQA (Class 1 – Existing Facilities).

**DISCUSSION OF PROJECT ISSUES**

*Compatibility:* In general, staff does not view the proposed auto body use as introducing any new significant issues considering the business would occupy an existing auto repair building that has been active at this location for 25 years. Staff would note that the building and vehicle bays face east, away from the adjoining mobile home park, specifically to mitigate potential impacts, such as noise, on residential uses. Provision of a spray booth inside the building would be subject to Fire Code requirements and other environmental regulation, similar to the handling of any hazardous materials (conditions of approval have been included in this regard). In addition, as described in the narrative, the body shop would have a lesser volume of deliveries and vehicles being serviced than the most recent tenant, McLea’s Tire, and thus generate less traffic and noise.

*Parking Adequacy:* As previously noted, the property is non-conforming with respect to the amount of on-site parking. The applicant is aware of this limitation and indicates that business operations can function acceptably with existing parameters as a practical matter. Parking for the business would continue to utilize existing parking areas on the south side of the property and in front of the office, and additional parking would be accommodated within the building given the two stall depth. The secured/fenced parking on the south side of the building can also be managed by staff to accommodate additional vehicles beyond the six striped spaces.

**RECOMMENDATION**

Staff recommends approval of the Use Permit subject to the attached conditions.

**Attachments**

1. Findings
2. Draft Conditions of Approval
3. Location Map
4. Project Narrative
5. Correspondence
6. Photos of Property
7. Site plans

cc: Shawn Crozat (via email)  
G & C Auto Body  
251 Bellevue Ave.  
Santa Rosa, CA 95407

Gary and Terrie Heon (via email)  
748 Elliot Street  
Sonoma, CA 95476

City of Sonoma Planning Commission  
**FINDINGS OF PROJECT APPROVAL**  
G & C Auto Body Repair Shop Use Permit  
19285 Sonoma Highway

August 13, 2015

Based on substantial evidence in the record, including but not limited to the staff report, and upon consideration of all testimony received in the course of the public review, including the public review, the City of Sonoma Planning Commission finds and declares as follows:

**Use Permit Approval**

1. That the proposed use is consistent with the General Plan and any Specific Plan;
2. That the proposed use is allowed with a conditional Use Permit within the applicable zoning district and complies with all applicable standards and regulations of the Development Code (except for approved Variances and Exceptions).
3. The location, size, design, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity; and
4. The proposed use will not impair the architectural integrity and character of the zoning district in which it is to be located.

City of Sonoma Planning Commission  
**CONDITIONS OF PROJECT APPROVAL**  
G & C Auto Body Repair Shop Use Permit  
19285 Sonoma Highway

August 13, 2015

1. The building and property shall be improved and used in conformance with the project narrative and approved site plan, except as modified by these conditions and the following.
  - a. Hours of operation for the auto body shop shall be limited to the hours between 8a.m. and 6p.m. Monday through Saturday.
  - b. The two storage containers currently located on the property shall be removed.
  - c. The parking area on the south side of the property shall be restriped in substantial conformance with the approved site plan.
  - d. No more than six employees shall work at the site at any one time.

*Enforcement Responsibility: Planning Department; Building Department*  
*Timing: Prior to operation; Ongoing*

2. On-site parking for customers, employees, and vehicles being serviced shall occur within designated parking spaces in substantial conformance with the approved site plan. Additional vehicles can be parked in tandem (or another configuration) inside the building and inside the secure/fenced parking area on the south side of the building. Vehicle parking shall be prohibited in front of the repair bays, within the access easement, or other vehicle circulation areas.

*Enforcement Responsibility: Planning Department; Fire Department*  
*Timing: Ongoing*

3. A building permit shall be required for the necessary tenant improvements. All Building Department requirements shall be met, including applicable Building Code requirements related to compliance with CALGreen and ADA standards (i.e. disabled access including at entrances, handicap parking, accessible paths of travel, bathrooms, etc.).

*Enforcement Responsibility: Building Department; Fire Department*  
*Timing: Prior to construction; Prior to operation*

4. All Fire Department requirements shall be met, including applicable requirements from Chapter 24 of the Fire Code based on the occupancy designation and details of the paint booth/spray finishing. An automatic fire sprinkler system shall be installed in the building if the cumulative total valuation of all building permits issued within any 36 month period exceeds \$150,000.

*Enforcement Responsibility: Fire Department; Building Department*  
*Timing: Prior to issuance of any building permit; Prior to operation*

5. The applicant shall obtain any necessary permits, licenses, and/or clearances from the Bay Area Air Quality Management District and other agencies or departments with jurisdiction over the auto body repair use and associated improvements, including hazardous materials, and the spray booth. Business operations and facilities shall conform to the requirements/limitations of those permits.

*Enforcement Responsibility: Agencies/Departments with Jurisdiction; Planning Dept.; Fire Dept.*  
*Timing: Prior to operation; Ongoing*

6. The following stormwater requirements shall apply to the use:
  - a. No person shall dispose of, nor permit the disposal, directly or indirectly of vehicle fluids, hazardous materials, or rinsewater from parts cleaning into storm drains.
  - b. No vehicle fluid removal shall be performed outside a building, except in such a manner as to ensure that any spilled fluid will be in an area of secondary containment. Leaking vehicle fluids shall be contained or drained from the vehicle immediately.

- c. No person shall leave open containers containing vehicle fluid unattended, unless such containers are in use or in an area of secondary containment

*Enforcement Responsibility:* Stormwater Coordinator; Public Works Director  
*Timing:* Ongoing

- 7. The applicant shall comply with the following requirements of the Sanitation Division of Sonoma County Permit & Resource Management Department (PRMD) and the Sonoma County Water Agency (SCWA) if deemed necessary:

- a. The applicant shall submit a Wastewater Discharge Survey to PRMD. The Applicant shall obtain a **Survey for Commercial/Industrial Wastewater Discharge Requirements** (“Green form”) from PRMD, and shall submit the completed Survey, along with two (2) copies of the project site plan, floor plan and plumbing plan to the Sanitation Section of PRMD. The Survey evaluation must be completed by the Sonoma County Water Agency and submitted to the PRMD Engineering Division before a building permit for the project can be approved.
- b. If additional sewer pre-treatment and/or monitoring facilities (i.e. Grease trap, Sampling Manhole, etc.) are required by the Sonoma Valley County Sanitation District per the Wastewater Discharge Survey, the Applicant shall comply with the terms and requirements of the Survey prior to commencing the use. If required, the Sampling Manhole shall be constructed in accordance with Sonoma County Water Agency *Design and Construction Standards for Sanitation Facilities*, and shall be constructed under a separate permit issued by the Engineering Division of PRMD.
- c. In accordance with Section 5.05, "Alteration of Use", of the Sonoma Valley County Sanitation District Ordinances, the Applicant shall pay increased sewer use fees as applicable for changes in the use of the existing structure. The increased sewer use fees shall be paid the Engineering Division of PRMD prior to the commencement of the use(s).
- d. A sewer clearance shall be provided to the City of Sonoma Building Department verifying that all applicable sewer fees have been paid prior to the issuance of any building permit. **Note: Substantial fees may apply for new sewer connections and/or the use of additional ESDs from an existing sewer connection. The applicant is encouraged to check with the Sonoma County Sanitation Division immediately to determine whether such fees apply.**

*Enforcement Responsibility:* Sanitation Division of Sonoma County Planning & Management Resource Department; Sonoma County Water Agency: City of Sonoma Building Department  
*Timing:* Prior to issuance of a building permit

- 8. Any exterior alterations to the building, including repainting, installation of an awning, and new entry doors, shall be subject to review and approval by the Design Review & Historic Preservation Commission (DRHPC).

*Enforcement Responsibility:* Planning Department; DRHPC  
*Timing:* Prior to any exterior building modifications

- 9. Signage for the business/property shall be subject to review and approval by City Staff or the Design Review & Historic Preservation Commission (DRHPC) as applicable.

*Enforcement Responsibility:* Planning Department or DRHPC  
*Timing:* Prior to installation of signage

# Vicinity Map

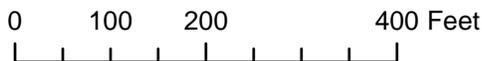
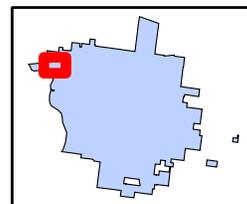


## Zoning Designations

- R-HS Hillside Residential (1 D.U./10acres, maximum)
- R-R Rural Residential (2 D.U./acre, maximum)
- R-L Low Density Residential (2-5 D.U./acre)
- R-S Sonoma Residential (3-8 D.U./acre)
- R-M Medium Denisty Residential (6-10 D.U./acre)
- R-H High Density (9-12 D.U./acre)
- R-O Housing Opportunity (15-20 D.U./acre)
- R-P Mobile Home Park (7 D.U./acre, maximum)
- MX Mixed Use (12 D.U./acre, maximum)
- C Commercial (15 D.U./acre, maximum)
- C-G Commercial-Gateway (15 D.U./acre, maximum)
- W Wine Production
- P Public Facility
- Pk Park
- A Agriculture

### Project Summary

<i>Project Name:</i>	G & C Auto Body Shop
<i>Property Address:</i>	19285 Sonoma Hwy.
<i>Applicant:</i>	G & C Auto Body
<i>Property Owner:</i>	Gary & Terrie Heon
<i>General Plan Land Use:</i>	Commercial
<i>Zoning - Base:</i>	Commercial
<i>Zoning - Overlay:</i>	N/A
<i>Summary:</i>	Consideration of a Use Permit to allow an auto body repair shop in an existing auto repair



1 inch = 200 feet

## Proposed use of 19285 Sonoma Highway for G&C Auto Body

We propose opening G&C Auto Body at 19285 Sonoma Highway to engage in collision repair. G&C Auto Body is a Sonoma County family owned and operated business since 1972. We invest heavily into our business between location image, equipment and training of staff with the aim of offering customers the best auto body experience. Our operation would be open Monday – Friday 8am – 6pm. We will be opening the business with 3 employees and with our market analyses see a maximum of five employees. We will provide parking for our 3 – 5 employees on our site and will likely use another 10 parking spaces for repairs in process. We will achieve this by parking behind the fence, removing all storage containers and parking inside the building. The building has double deep stalls which do not have a use for us since technicians can only work on one car at a time. This will allow us extra parking in the building and give us more parking than we need. We will be receiving much less deliveries than McLea's Tire (probably 4 -8 a day) and will provide a parking spot for deliveries to prevent congestion to the carwash. Our average repair order is \$3,000 and we will likely repair 2 to 3 cars a day. In the tire/automotive business like McLea's they have a much lower repair order average (\$400 - \$500) and therefore do much more volume than we do. Because we do a lower volume of work we will have less customers coming in, need less parking and have less deliveries therefore creating less congestion and noise. We use similar air tools as McLea's but in our industry we repair a lot of panels unlike tires/automotive where they replace almost everything (tires, brake pads, shocks, etc.). Because we repair more we use less air tools therefore creating less noise than tires/automotive. We would be using a permitted spray booth to prevent any paint fumes entering the environment and use both Safety Kleen (hazardous waste removal) and GMG Enviro (consultant) to ensure that we are compliant with all hazardous waste. We propose painting the building earth tones (tan and green), adding an awning, replacing the front door and remodeling the office. We are really excited about the opportunity to service the Sonoma community. Thank you.

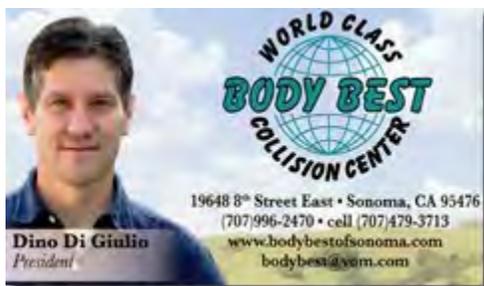
## Rob Gjestland

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**From:** Body Best of Sonoma <bodybest@vom.com>  
**Sent:** Wednesday, July 29, 2015 1:01 PM  
**To:** Rob Gjestland  
**Subject:** Re: G & C Auto Body Application for 19285 Sonoma Hwy

He left out one thing the overspray from the paint booth and the house right behind the shop also they will grow bigger they are good at marking and will take over more space down the road if Sonoma is looking for that there's nothing I can say. It will effect my business some. If any one from the board would like to see a body shop running I would love for them to stop by. The traffic on some days will be more then the tire shop they will be a drive in for some big insurance company's that could be 3 to 8 cars a day more then cars they are repairing. Thanks for the help if I can do anything let me know.

Yours truly  
Dino DiGiulio



**From:** [Rob Gjestland](#)  
**Sent:** Wednesday, July 29, 2015 11:35 AM  
**To:** [bodybest@vom.com](mailto:bodybest@vom.com)  
**Subject:** G & C Auto Body Application for 19285 Sonoma Hwy

Hi Dino:

G&C's application is attached for your consideration.

Thanks,

Rob Gjestland  
Senior Planner  
City of Sonoma  
(707) 933-2202



**We Listen. We Promise. We Deliver.**

RECEIVED

JUL 31 2015

CITY OF SONOMA

7/22/2015

Dear Occupant,

My name is Shawn Crozat and I am the Chief Operations Officer and partial owner of G&C Auto Body. We are in the process of opening a new location at 19285 Sonoma Highway, Sonoma. G&C Auto Body is a Sonoma County family owned and operated business that was started in 1972 by my Dad. As a good neighbor I wanted to reach out to address any concerns you might have. We have nine locations and will manage our Sonoma location with the same care and concern as we do our others. Due to the nature of our business we will have less volume in business than the previous tire shop and muffler shop. To our neighbors that mean less traffic and less noise than previous businesses at that site. We are environmentally responsible and are committed to the communities we serve. If you have any questions or concerns please contact me directly on my cell at 707-217-0999 or by email at [scrozat@gandcautobody.com](mailto:scrozat@gandcautobody.com). Thank you.

Sincerely,

*Shawn Crozat*

Shawn Crozat

MHP = MOBILE HOME PARK  
RDS = RANCHO DE SONOMA

DEAR MR. CROZAT,  
THANKS FOR THE LETTER OF INTRODUCTION. MY NAME IS  
JEFFREY A FERRERO & I LIVE AT 4 RAMON WHICH IS IN THE  
RANCHO DE SONOMA MOBILE HOME PARK & I AM DIRECTLY  
ACROSS THE ST. FROM YOUR NEW LOCATION. I IMMEDIATELY  
RECOGNIZED THE NAME OF YOUR COMPANY & I REALIZE YOU ARE LONG  
TIME SONOMA COUNTY BUSINESS. I'M SURE YOU WILL BE A GOOD NEIGH-  
BOR. I HAVE LIVED HERE SINCE 1995 & G&C WILL BE THE 4TH TENANT  
SINCE 95.

PARKING IN THIS AREA IS TIGHT BECAUSE OF THE 2 ADJACENT CAR  
WASHES, GAS STATION EMPLOYEES ALL PARK ON RAMON BEFORE IT  
ENTERS THE MOBILE HOME PARK & WHAT INVARIABLY HAPPENS IS  
THAT PEOPLE PARK THEIR CARS IN THE MHP. SINCE RAMON IS A NARROW  
ST. WITH NO SIDEWALKS THIS CAN CAUSE A SAFETY HAZARD & SINCE RDS/MHP IS

OVER

A SENIOR COMMUNITY WE SEE AMBULANCES & FIRE TRUCKS QUITE OFTEN. WHEN PEOPLE PARK IN THE MHP IT CREATES A SAFETY HAZARD.

I'VE SEEN IT HAPPEN BEFORE WITH NEW TENANTS AT 19285 SON. HWY. ONE PERSON PARKS IN THE MHP & IT BECOMES A MONKEY SEE MONKEY DO SITUATION. I'VE SEEN PEOPLE PARK IN BOTH DIRECTIONS & IT BECOME UNPASSABLE FOR EMERGENCY VEHICLES. SO IF YOU COULD ASK YOUR EMPLOYEES NOT TO PARK IN THE MHP IT WOULD BE GREATLY APPRECIATED & CUT DOWN ON SAFETY HAZARDS.

AS I SAID I'M SURE YOU WILL BE A GOOD NEIGHBOR - I THOUGHT I WOULD RESPOND & ALSO SEND A COPY OF MY LETTER TO THE SONOMA COUNTY CITY PLANNING COMMISSION.

WITH BEST REGARDS

Amy A. Ferrero

11 RAMON ST.

SONOMA CA 95476

P.S. I SHOULD MENTION THAT I AM IN NO WAY CONNECTED TO THE MANAGEMENT OR OWNER SHIP OF RANCHO DE SONOMA. I A LONG TIME RESIDENT CONCERNED ABOUT POTENTIAL SAFETY HAZARDS IN OUR SENIOR COMMUNITY

RECEIVED

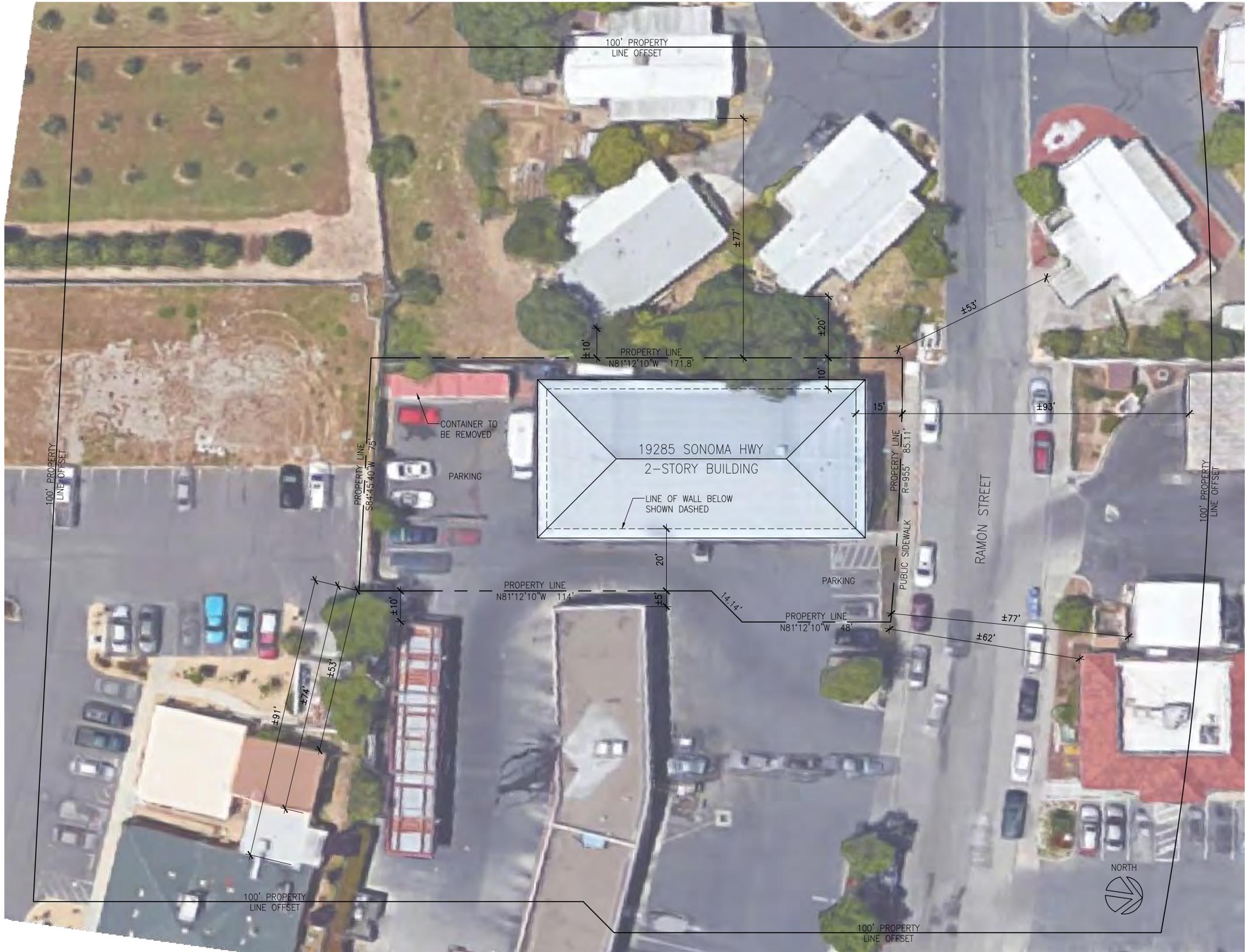
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CITY OF SONOMA







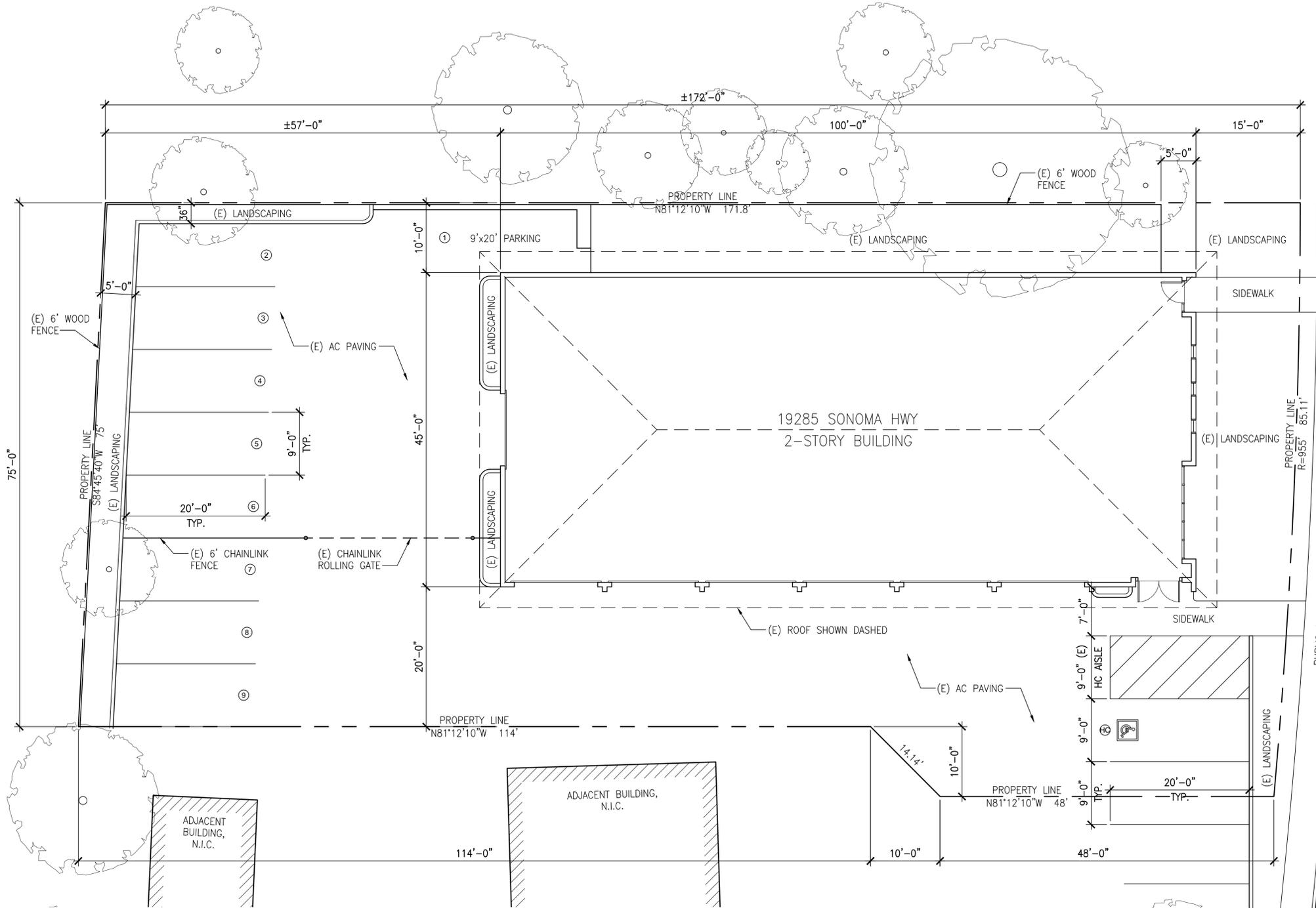


1 PROXIMITY SITE PLAN  
 A1.1 SCALE: 1/16" = 1'-0"

Revisions	07-17-2015
USE PERMIT SUBMITTAL	

Date:	7-17-15
Scale:	As Noted
Drawn:	JMK
Job #	15037
Prototype	DIVINE

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1 SITE PLAN  
 A1.2 SCALE: 1/8" = 1'-0"

**SITE AREA TABULATIONS**

LOT AREA =	13,500 SF	PAVED AREA =	6,620 SF
BUILDING FOOTPRINT =	4,500 SF	LANDSCAPING =	2,380 SF
LOT COVERAGE =	33.3%		
BUILDING AREA =	6,385 SF		
F.A.R. RATIO =	47.3%		

EXISTING PARKING PROVISIONS:  
 (9) 9'x20' STANDARD STALLS  
 (1) ACCESSIBLE STALL

Revisions

07-17-2015	USE PERMIT SUBMITTAL
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Date:	7-17-15
Scale:	As Noted
Drawn:	JMK
Job #	15037
Prototype	DIVINE

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**Agenda Item Title:** Application for a Use Permit to allow vocational specialty businesses within a mixed-use building.

**Applicant/Owner:** EVAC (dba Extraordinary Ventures of California)/Peak Napa Street Associates, LLC

**Site Address/Location:** 430 West Napa Street, Suites E and F

**Staff Contact:** Wendy Atkins, Associate Planner  
Staff Report Prepared: 07/30/15

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### **PROJECT SUMMARY**

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**Description:** Application of EVAC for a Use Permit to allow vocational specialty businesses within a mixed-use building located at 430 West Napa Street, Suites E and F.

**General Plan Designation:** Mixed Use

**Zoning:** **Base:** Mixed Use (MX) **Overlay:** None

**Site Characteristics:** The property is a ±13,150 square foot lot located on the north side of West Napa Street near the corner of West Napa Street and Fourth Street West. The property is currently developed with a two-story building with office/commercial space on the ground floor and four residential units on the second floor.

**Surrounding Land Use/Zoning:** **North:** Single-family home/ Low Density Residential (R-L)  
**South:** Multi-family homes/ Commercial (C)  
**East:** Office building/ Mixed Use (MX)  
**West:** Single-family home/ Mixed Use (MX)

**Environmental Review:**

<input checked="" type="checkbox"/> Categorical Exemption	<input type="checkbox"/> Approved/Certified
<input type="checkbox"/> Negative Declaration	<input checked="" type="checkbox"/> No Action Required
<input type="checkbox"/> Environmental Impact Report	<input type="checkbox"/> Action Required
<input type="checkbox"/> Not Applicable	

**Staff Recommendation:** Approve, subject to conditions.

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## **PROJECT ANALYSIS**

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### **DETAILED PROJECT DESCRIPTION**

The applicant is requesting Use Permit approval to allow four vocational specialty businesses to operate within the mixed-use building. The proposed businesses would occupy the first floor tenant space and consist of the following:

- Pansy Creations (a privately-owned business that creates acrylic paintings, ceramics, and fabric arts);
- EVAC Office Solutions (a support service that specializes in providing letter-shop packaging, assembly, and related services to small and mid-sized businesses);
- EVAC Laundry (personal services that provides premium-quality wash, dry, and fold services with free pick-ups and delivery); and,
- EVAC Pets (a professional dog walking and cat sitting service).

EVAC is a nonprofit organization that creates and nurtures self-sustaining small businesses designed around the skills of young adults with autism and developmental disabilities. To accommodate the change in use, the 4,222 square foot ground floor area would be remodeled by removing and relocating interior walls, adding additional plumbing for four washer and dryer units, and adding a small kitchen area for employee use.

### **GENERAL PLAN CONSISTENCY** ( **Not Applicable to this Project**)

The property is designated Mixed Use by the General Plan, which permits commercial and office uses subject to use permit approval in order to ensure compatibility with adjacent neighborhoods. The proposal does not raise any issues in terms of consistency with regard to General Plan goals and policies.

### **DEVELOPMENT CODE CONSISTENCY** ( **Not Applicable to this Project**)

*Use:* The property is zoned Mixed Use (MX). The property is designated Mixed Use by the General Plan. The Mixed Use land use designation is intended to accommodate uses that provide a transition between commercial and residential districts, to promote a pedestrian presence in adjacent commercial areas, and to provide neighborhood commercial services to adjacent residential areas. In the MX zone, commercial and office uses are allowed, subject to review and approval of a Use Permit by the Planning Commission.

*Development Standards:* The proposed use would operate within an existing building and new construction would be limited to an interior remodel. As a result, the project does not raise any issues in terms of compliance with building setback, FAR, lot coverage, open space, and building height standards.

*Parking:* The Development Code provides that the Planning Commission determines the appropriate amount of parking for a mixed use project and it also allows for the potential reduction in the amount of required parking when it is found that different uses served by the parking have different peak parking periods. The parking requirement for multi-family dwellings is one and one-half spaces for each unit, with one space for each unit covered, plus guest parking at the rate of 25% of total required spaces. The parking requirement for laundromats, offices, and retail sales is one space for each 300 square foot of gross floor area. Based on the proposed mix of uses within the building, 22 spaces would be required under those ratios, versus the 19 parking spaces that are provided on the site. While it is up to the Planning Commission to determine if adequate parking exists on the site to support the new mix of uses, staff would note that the 19-stall parking lot previously supported a health care center use, in addition to

the four apartments. In staff's view, the proposed ground-floor use is less intense than the health care center. In addition, because the ground-floor uses and the apartments will have different peak parking demands, it seems reasonable to assume that guest parking for the apartments will be available on evenings and weekends.

*Bicycle Parking:* Any change of commercial use within an existing structure must provide bicycle parking (§19.48.110). The applicant has indicated that bicycle parking would be provided in the form of one inverted U bicycle rack located at the rear of the building near the accessible parking space. This requirement has been reflected in the draft conditions of approval (Condition of approval number 5).

**CONSISTENCY WITH OTHER  
CITY ORDINANCES/POLICIES** (Not Applicable to this Project)

**ENVIRONMENTAL REVIEW** (Not Applicable to this Project)

Pursuant to Section of 15301 of the State CEQA Guidelines, the leasing, permitting, or operation of existing private structures involving negligible or no expansion of use is considered Categorically Exempt from the provisions of CEQA (Class 1 – Existing Facilities).

**DISCUSSION OF PROJECT ISSUES**

*Compatibility with Surrounding Land Uses:* The property is located in a Mixed Use zoning district with a variety of land uses in the vicinity, including adjoining residential uses to the west and north. Aside from issues related to parking (discussed above) the proposal needs to be considered in terms of compatibility with these nearby uses. While staff has not identified any significant compatibility issues associated with the proposed businesses, in order to minimize potential conflicts related to hours of operation and noise, the project narrative states that deliveries and hours of operation shall be limited to 9 a.m. to 5 p.m. Monday through Friday and this requirement has been included in the attached draft condition of approval (see condition number 1).

**RECOMMENDATION**

Staff recommends approval, subject to the attached conditions.

**Attachments**

1. *Draft Findings of Project Approval*
2. *Draft Conditions of Approval*
3. *Location Map*
4. *Correspondence*
5. *Project Narrative*
6. *Site Plan*
7. *Proposed Floor Plan*
8. *Current Floor Plan*
9. *Existing Elevations*

cc: EVCA, Inc.  
430 West Napa Street, #F  
Sonoma, CA 95476

Peak Napa Street Associates, LLC  
2 Catskill Court  
San Anselmo, CA 94960

Peak Napa Street Assoc LLC  
300 Drakes Landing RD. #250  
Greenbrae, CA 94904-3125

City of Sonoma Planning Commission  
**FINDINGS OF PROJECT APPROVAL**  
EVCA Conditional Use Permit – 430 West Napa Street

August 12, 2015

Based on substantial evidence in the record, including but not limited to the staff report, and upon consideration of all testimony received in the course of the public review, including the public review, the City of Sonoma Planning Commission finds and declares as follows:

**Use Permit Approval**

1. That the proposed use is consistent with the General Plan and any Specific Plan;
2. That the proposed use is allowed with a conditional Use Permit within the applicable zoning district and complies with all applicable standards and regulations of the Development Code (except for approved Variances and Exceptions).
3. The location, size, design, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity; and
4. The proposed use will not impair the architectural integrity and character of the zoning district in which it is to be located.

City of Sonoma Planning Commission  
**CONDITIONS OF APPROVAL**  
**FINDINGS OF PROJECT APPROVAL**  
EVCA Conditional Use Permit – 430 West Napa Street

August 12, 2015

1. The project shall be constructed and operated in conformance with the approved site plan, floor plan, and building elevations except as modified by these conditions. The hours of operation, including deliveries, shall be limited to the following hours: 9 a.m. to 5 p.m. Monday through Friday.

*Enforcement Responsibility: Planning, Building and Public Works*  
*Timing: Prior to issuance of a building permit; Ongoing*

2. All Building Division requirements shall be met, a building permit shall be required.

*Enforcement Responsibility: Building Division*  
*Timing: Prior to construction*

3. All Fire Department requirements shall be met.

*Enforcement Responsibility: Fire Department; Building Division*  
*Timing: Prior to issuance of a building permit*

4. The applicant shall obtain any necessary approvals from the Sonoma County Environmental Health Division for the laundry element.

*Enforcement Responsibility: Building Division; Sonoma County Environmental Health Division.*  
*Timing: Prior to operating and/or issuance of occupancy permit*

5. One inverted U bicycle parking rack shall be provided, which will accommodate two bicycles.

*Enforcement Responsibility: Planning Department;*  
*Timing: Prior to the issuance of a building permit*

# Vicinity Map



## Zoning Designations

- R-HS Hillside Residential (1 D.U./10 acres, maximum)
- R-R Rural Residential (2 D.U./acre, maximum)
- R-L Low Density Residential (2-5 D.U./acre)
- R-S Sonoma Residential (3-8 D.U./acre)
- R-M Medium Density Residential (6-10 D.U./acre)
- R-H High Density (9-12 D.U./acre)
- R-O Housing Opportunity (15-20 D.U./acre)
- R-P Mobile Home Park (7 D.U./acre, maximum)
- MX Mixed Use (12 D.U./acre, maximum)
- C Commercial (15 D.U./acre, maximum)
- C-G Commercial-Gateway (15 D.U./acre, maximum)
- W Wine Production
- P Public Facility
- Pk Park
- A Agriculture

### Project Summary

*Project Name:* EVAC Use Permit

*Property Address:* 430 West Napa Street

*Applicant:* EVAC

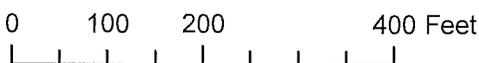
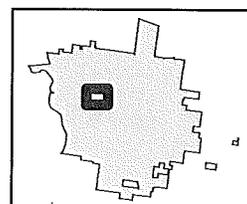
*Property Owner:* Peak Napa Street

*General Plan Land Use:* Mixed Use

*Zoning - Base:* Mixed Use

*Zoning - Overlay:* N/A

*Summary:*  
 Consideration of a Use Permit to allow vocational specialty businesses within a mixed-use building.



1 inch = 200 feet

RECEIVED

JUL 15 2015

CITY OF SONOMA

**USE PERMIT APPLICATION**  
**Project Narrative**  
July 15, 2015

**Property Address - 430 West Napa Street, Sonoma, CA**

**Applicant – EVCA, Inc.**

**Property Owner – Peak Napa Street Associates, LLC**

***General Overview***

EVCA, Inc. (dba Extraordinary Ventures of California) is leasing the 4,222 square foot ground floor commercial space located at 430 West Napa Street in Sonoma. This use permit application is for the proposed uses for the space. EVCA is requesting approval for the following uses.

<b>PROPOSED LAND USE</b>	<b>Notes</b>
Retail Trade - Artisan Shops	Pansy Creations is a privately-owned business that creates acrylic paintings, ceramics, and fabric arts
Services Business Support Services	EVCA Office Solutions specializes in providing letter-shop, packaging, assembly and related services to small and mid-sized businesses
Services - Personal Services	EVCA Laundry provides premium-quality wash, dry and fold service with free pick-ups and delivery – 4 residential washer/dryer units are projected to be installed  EVCA Pets is a professional, courteous and warmhearted dog walking and cat sitting service – animals are not kept on site

***Description of Applicant***

EVCA is a nonprofit organization that creates and nurtures self-sustaining small businesses designed around the skills of the young adults with autism and developmental disabilities that serve as its workforce. We have affiliated with Extraordinary Ventures headquartered in Chapel Hill, NC which has received national acclaim for its pioneering entrepreneurial business model and has appeared on CNBC and in USA Today. We plan to start accepting applications for employment as soon as August 1, 2015.

EVCA's businesses and jobs will be chosen, developed and designed specifically to suit the needs, skills and interests of the employees. Each business will be treated as a real business—rather than a traditional social program—and will be managed at all times on sound, professional business concepts

and practices. We will create meaningful employment that leads to an improvement of work skills, pride and personal satisfaction for the employees. Initial businesses may include a wash & fold laundry service, an office solutions operations and a dog-walking business (no animals on-site)

EVCA, Inc. will engage in the following activities.

- Provide vocational, training and social opportunities for (a) students and adults with developmental disabilities who are transitioning from a high school setting and (b) adults with developmental disabilities an opportunity to work in the community
- Develop various vocational and volunteer opportunities for individuals who might not otherwise seek out employment
- Support the training and placement of adults by offering a variety of jobs and volunteer placements in the community
- Modify and adapt appropriate job opportunities to fit the learning style and interests of the employee.
- Promote and collaborate with public, private and religious agencies/companies to increase the company's customer database.
- Promote independence, self-worth and choice to allow individuals with developmental disabilities to have greater control of their lives
- Create multiple self-sustaining businesses that employ individuals with developmental disabilities within the community
- Support individuals in their own micro-enterprises and self-employment opportunities

### ***Specific Project Data***

The Property – The site is the former location of the Sonoma Valley Community Health Center which vacated the space in mid-2014. EVCA will be making minor modifications to the interior space – primarily removing/moving interior walls and adding additional plumbing for 4 washer/dryer units. Standard tenant improvements will include new carpet, paint and any ADA accommodations as required by code. A small kitchen will be added for employee use. Exterior signage will be designed and installed after approval of Sonoma's Design Review Board.

Zoning District - MX (Mixed Use) - The MX zoning district is intended to allow for higher density housing types, such as apartments and condominiums, in conjunction with commercial and office development, in order to increase housing opportunities, reduce dependence on the automobile, and provide a pedestrian presence in commercial areas.

Building Area Summary – The building is 2 stories with office/commercial space on the ground floor and four (4) residential units on the 2<sup>nd</sup> floor. The residential units are 2 one-bedroom and 2 two-bedroom units.

Building Height - 20 feet to roof line (30 feet permitted by code)

Lot Area – 13,650 square feet

Building Coverage – 4,222 square foot (ground floor footprint)

Lot Coverage – 31% (60% permitted by code)

Total Building Area (gross) – 8,444 square feet (net square feet – approx. 7,500 sf)

Floor/Area Ratio – 0.62 (0.70 permitted by code)

Number of Employees – 15 (future)

Hours of Operation – 9:00AM – 5:00PM / Monday - Friday

Shipping & Delivery Schedule – 9:00AM – 5:00PM / Monday - Friday

Drive-through Service - None

Outdoor Storage Needs – None

Exhibits Attached

Exhibit A – Location Map

Exhibit B – Site Plan

Exhibit C – Current Floor

Exhibit D – Proposed Floor Plan

Exhibit E – Building Elevations

Contact Information:

Mark Jackson

EVCA, Inc.

430 West Napa Street, Suite F

Sonoma, CA 95476

707-781-1881

[mark@evcalifornia.org](mailto:mark@evcalifornia.org)

Mr. David Goodison  
Planning Director  
City of Sonoma  
No. 1, The Plaza  
Sonoma, CA 95476

**RE: Support for EVCA, Inc. Use Permit Application**

Dear Mr. Goodison:

I am a neighbor of 430 West Napa Street. I am writing to express my support of the proposed conditional use permit for EVCA, Inc. dated 7-15-15 (dba Extraordinary Ventures of California) at that location.

The applicant has provided me with a copy of the application, the project narrative for the use permit, the site plan and proposed floor plan. The project appears to be compatible with neighboring uses and should not have a material effect on the neighborhood.

In addition, the new businesses being created by EVCA, Inc. will employ persons with developmental disabilities and be an overall benefit to the Sonoma community. The unemployment rate for persons with developmental disabilities exceeds 85% and EVCA is doing something creative to help address this serious problem.

Thank you for your attention. Please contact me with any questions concerning this letter.

Sincerely, *Joseph D. Miccio*

Name: Joseph D. Miccio

Address: 430 W. NAPA ST. APT. C

City/State: SONOMA, CA, 95476

Phone: 707-938-3176

Email: \_\_\_\_\_

RECEIVED  
AUG 05 2015  
CITY OF SONOMA

Mr. David Goodison  
Planning Director  
City of Sonoma  
No. 1, The Plaza  
Sonoma, CA 95476

**RE: Support for EVCA, Inc. Use Permit Application**

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In addition, the new businesses being created by EVCA, Inc. will employ persons with developmental disabilities and be an overall benefit to the Sonoma community. The unemployment rate for persons with developmental disabilities exceeds 85% and EVCA is doing something creative to help address this serious problem.

Thank you for your attention. Please contact me with any questions concerning this letter.

Sincerely,

Name: Dee Corti

Address: 3998 White Oak Ct.

City/State: Sonoma

Phone: 996-3019

Email: \_\_\_\_\_

JUL 20 2015

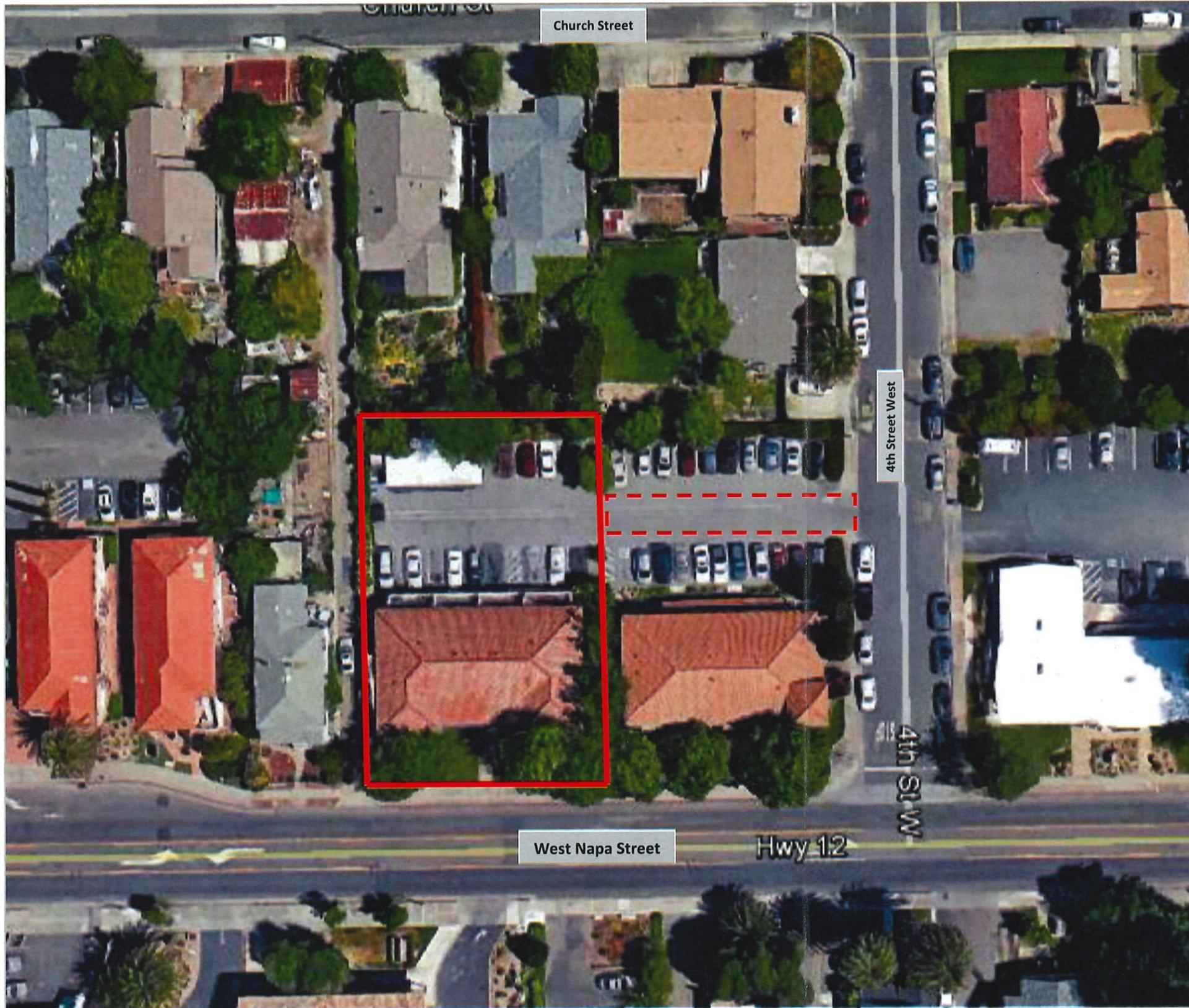


Exhibit A  
Location Map  
430 West Napa Street  
Use Permit Application



Property Boundary   
Access Easement 

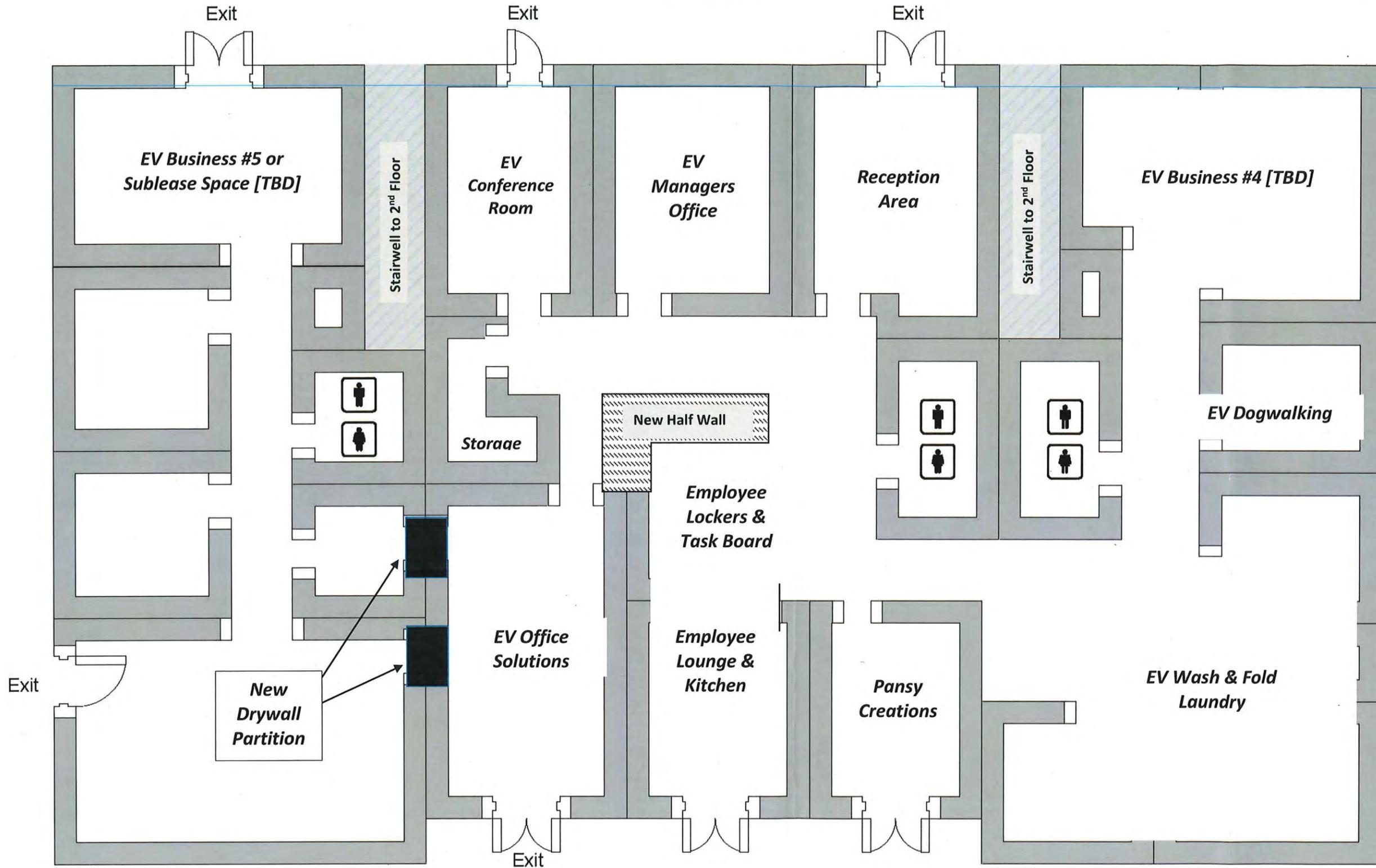


North

Scale 1" = 45'  


JUN 15 2015

PARKING AREA



TO WEST NAPA STREET

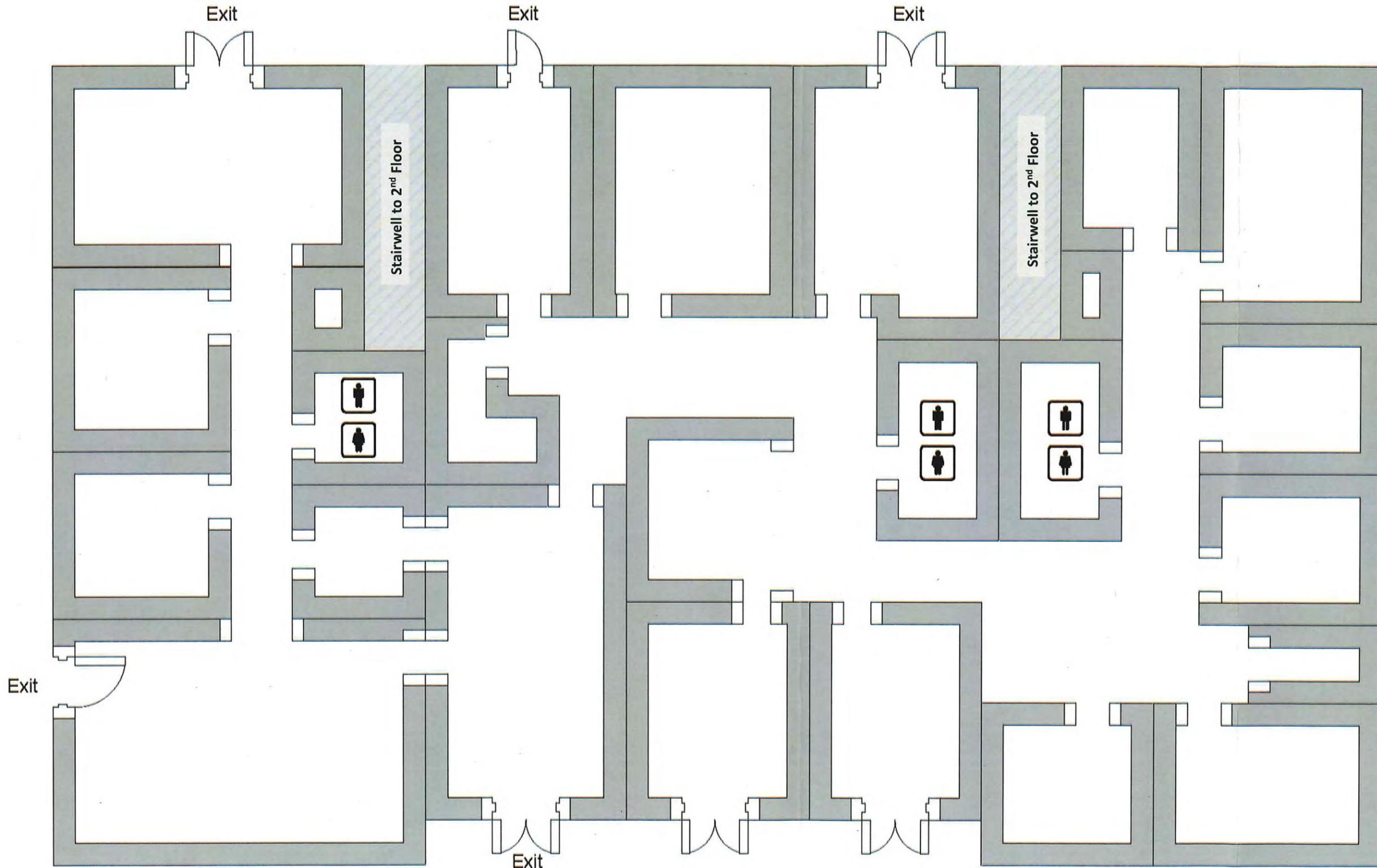


Scale  
1" = 6.25'

**EXHIBIT D**  
**PROPOSED PLAN**  
**GROUND FLOOR**

JUN 15 2015

PARKING AREA



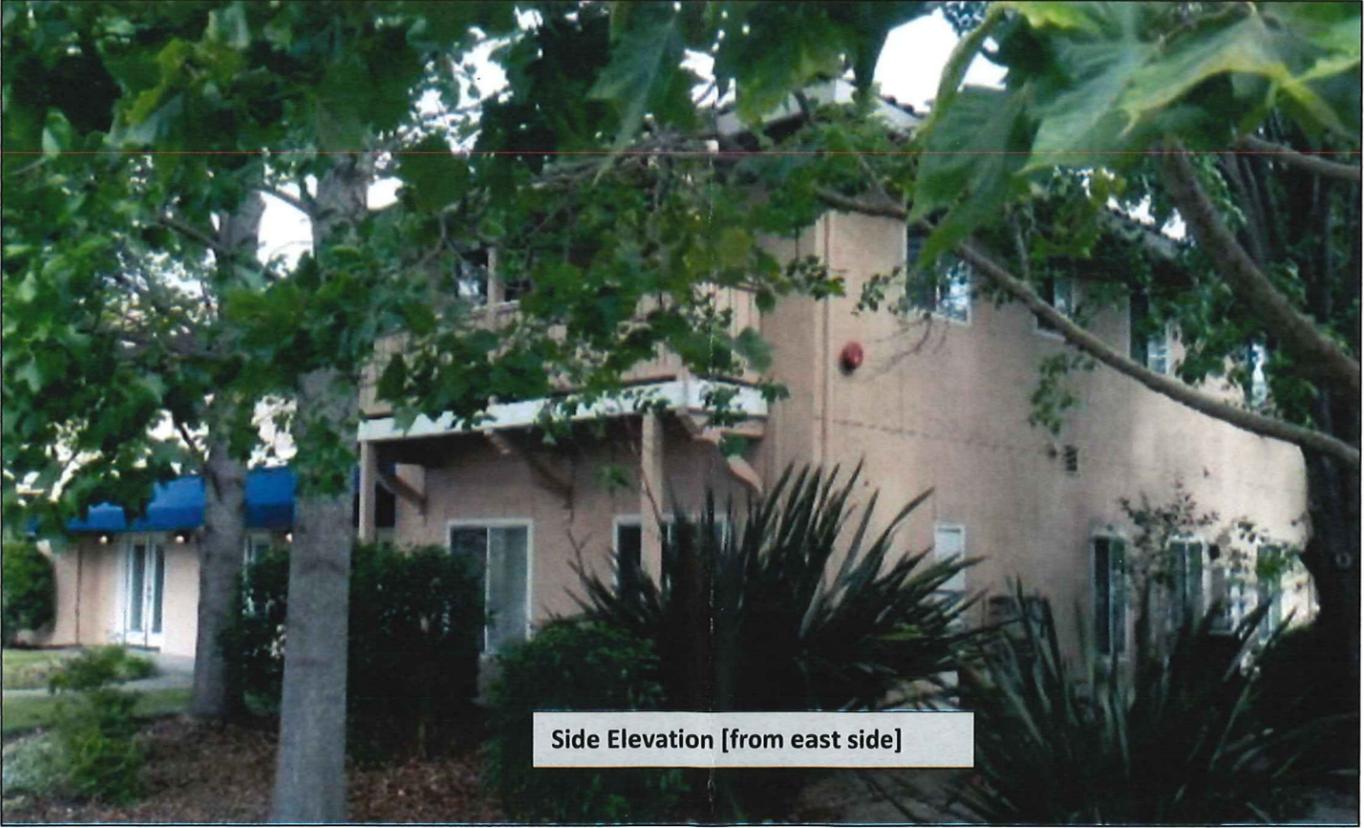
**EXHIBIT C**  
**CURRENT PLAN**  
**GROUND FLOOR**  
 [former Sonoma Valley Health Center]

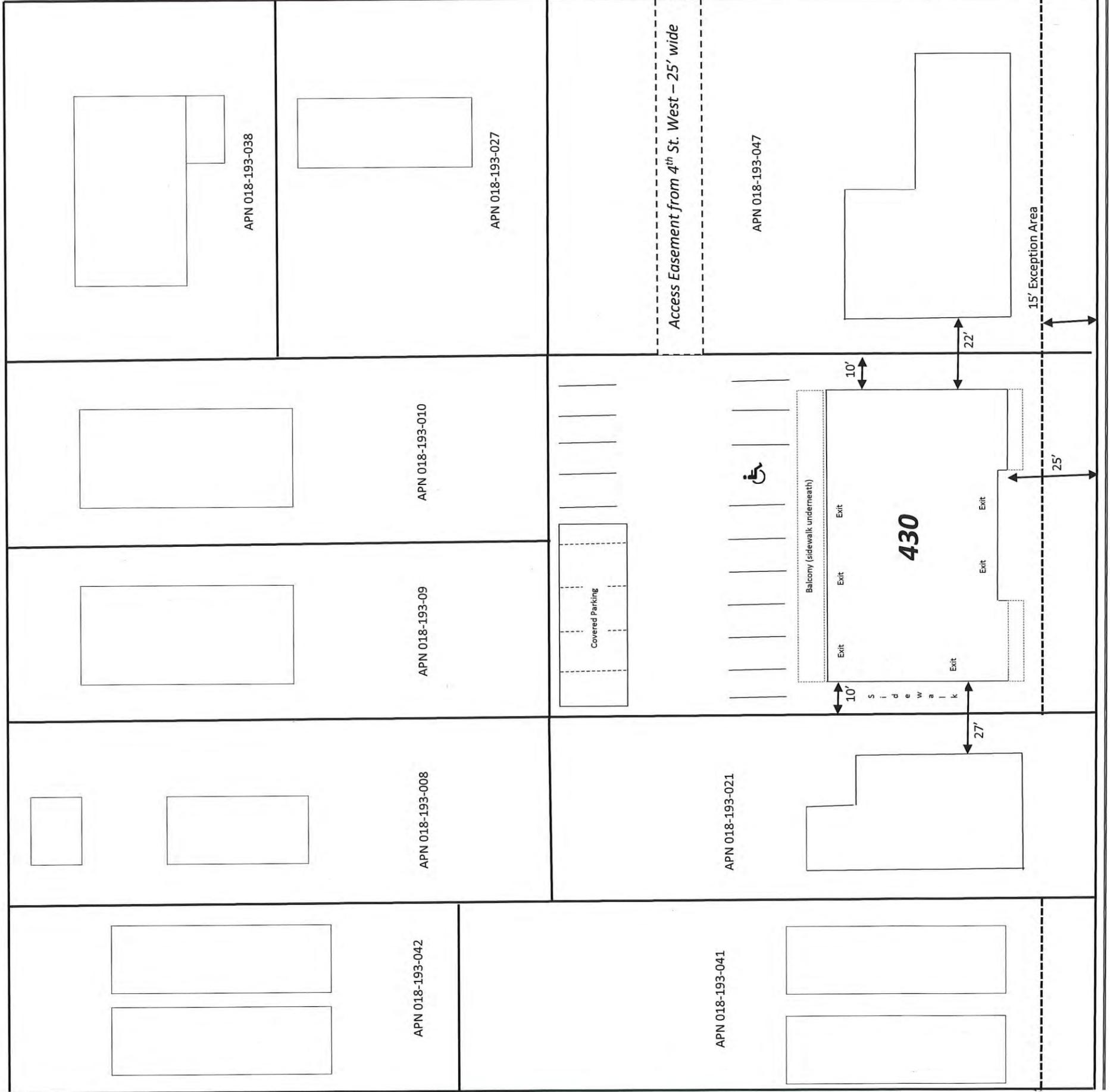
TO WEST NAPA STREET  
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Scale  
 1" = 6.25'

JUN 15 2015

Exhibit E  
Exterior Elevations  
430 West Napa Street

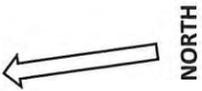




PUBLIC SIDEWALK

WEST NAPA STREET

Scale 1" = 30'



**Project Site - 430 West Napa Street**

APN - 018-193-048	
Zoning - MX (Mixed Use)	
Floors - 2	
Lot Dimension - 100' x 135' = 13,500 SF	
Lot Coverage - 4,222sf / 13,500sf = 0.31	
Residential Area (4 units) - 4,222 sf	
Commercial Area - 4,222 sf	
Total Bldg Area - 8,444 sf	
Floor-Area Ratio - 0.62	
<b>Parking</b>	
Handicap Access	1
Covered	4
Open	15
<b>Total Spaces</b>	<b>19</b>



Exhibit B

JUN 15 2015

## MEMO

**To:** Planning Commission  
**From:** David Goodison, Planning Director  
**Subject:** Revised Application of Richard Konecky for an Exception to the garage setback requirements to enclose a carport under construction at 753 Third Street East

### Background

On August 14, 2014, the Planning Commission approved an exception from the side yard setback requirements to substantially remodel and add onto the residence at 753 Third Street East. The approved project design included an attached carport on the north side of the property in line with the front of the home. In February 2015, while the project was under construction, and the applicant applied for an Exception to enclose the carport as a garage. Although the applicant was able to demonstrate broad neighbor support for the proposal, it was not well received by the Planning Commission. Some Commissioners noted that they were taken by surprise by the removal and reconstruction of most of the residence, as they had viewed the proposal as a remodeling that was therefore subject to greater design constraints, which justified the side-yard setback Exception. At least one Commission expressed the view that the proposal amounted to “piecemeal planning”, in which one Exception approval is used to justify a subsequent proposal. Ultimately, the Commission voted 5-0 to deny the application. This decision was not appealed. Although this limitation is not specifically set forth in the Development Code, staff discourages the re-application for a proposal that has been denied, especially during the first 12 months following the decision. In this instance, the applicant has made some changes to the previous proposal and has raised a second and previously un-discussed alternative, so staff has brought this matter back to the Planning Commission.

### Garage Setback Exception

The applicant would like to modify the approved plan to enclose the attached carport and create a garage for secure vehicle parking/storage and aesthetic considerations. However, the property’s R-L zoning requires garages to be setback 20 feet from the face of the residence (a provision that does not apply to open carport structures). Because the new carport is in line with the front of the home, enclosing it is subject to Planning Commission review of an Exception from the garage setback standard. The findings required for approval of an Exception are set forth below:

1. *The adjustment authorized by the Exception is consistent with the General Plan, any applicable Specific Plan, and the overall objectives of this Development Code;*
2. *An exception to the normal standards of the Development Code is justified by environmental features or site conditions; historic development patterns of the property*

*or neighborhood; or the interest in promoting creativity and personal expression in site planning and development;*

3. *Granting the Exception will not be detrimental to the public health, safety, or welfare, or injurious to the property or improvements in the vicinity and in the same zoning district.*

The project narrative sets forth the following arguments in support of the revised proposal:

Creating a garage in line with the front of the home would be consistent with conditions in the cul-de-sac as most homes have forward or in-line garages (residences in the neighborhood were constructed prior to adoption of the garage setback standard). The narrative includes a map and photographs illustrating conditions within the cul-de-sac.

- A number of property owners/residents within the cul-de-sac continue to prefer an enclosed garage. No neighbor has expressed opposition.
- The design of the conversion has been modified by the placement of windows on the face of the garage and on the north elevation (see attached elevations).
- The residence was broken into in September 2014 and the applicant is concerned that the carport creates a security issue.

Although these circumstances weigh in favor of the proposed modification, as previously discussed the remodel project was originally designed and presented to the Planning Commission with a carport to avoid the garage setback requirement or another exception. In addition, the original home was conforming in this regard. Staff's evaluation of the side yard setback request in August 2014 suggested that the proposal would result in more building mass across the front of the property than typical of conditions within the cul-de-sac, but that increase would be somewhat offset by the open carport feature. That being said, the applicant explains that the original plan was rushed and/or misguided by the designer, which resulted in reconsideration of this matter during the construction phase.

A second alternative, presented to staff by the applicant's contractor, is the concept of installing a metal gate on the front-facing opening of the carport, leaving the north opening clear. The gate would be solid for the first six feet in height, but would feature lattice-like open-work for the remainder. (Note: a specific design has not been provided.) In the Development Code, a carport is defined as "*carport is an attached or detached accessory structure enclosed on no more than two sides.*" If the Planning Commission is interested in this alternative, it would need to decide if the use of a gate would still allow the carport to be defined as such, or, whether approval of an Exception would still be necessary.

## **Recommendation**

Commission discretion.

Attachments

1. *Findings*
2. *Draft Conditions of Approval*
3. *Vicinity Map*
4. *Planning Commission Minutes, February 12, 2015*
5. *Project Narrative (includes site plan and elevations)*
6. *Correspondence/Letters of Support (Recent and Previous)*

cc: Matt McGinty (via email)  
950 Harley Street  
Sonoma, CA 95476

Richard Konecky (via email)  
1000 Chestnut St. #4B  
San Francisco, CA 94109

City of Sonoma Planning Commission  
**FINDINGS OF PROJECT APPROVAL**  
Konecky Garage Setback Exception – 753 Third Street East

August 13, 2015

Based on substantial evidence in the record and upon consideration of all testimony received in the course of the public review, including the public review, the City of Sonoma Planning Commission finds and declares as follows:

**Exception Approval:**

1. The adjustment authorized by the Exception is consistent with the General Plan, any applicable Specific Plan, and the overall objectives of this Development Code;
2. An exception to the normal standards of the Development Code is justified by environmental features or site conditions; historic development patterns of the property or neighborhood; or the interest in promoting creativity and personal expression in site planning and development;
3. Granting the Exception will not be detrimental to the public health, safety, or welfare, or injurious to the property or improvements in the vicinity and in the same zoning district.

City of Sonoma Planning Commission  
**CONDITIONS OF PROJECT APPROVAL**  
Konecky Garage Setback Exception – 753 Third Street East

August 13, 2015

1. Conversion of the carport into a garage shall be constructed in conformance with the project narrative, approved site plan and building elevations.

*Enforcement Responsibility: Planning Department; Building Department*

*Timing: Prior to construction; Prior to final occupancy*

2. All Building Department requirements shall be met. A design change application/building permit shall be required.

*Enforcement Responsibility: Building Department*

*Timing: Prior to construction*

# Vicinity Map

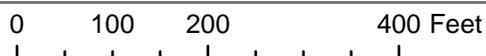


## Project Summary

<i>Project Name:</i>	Konecky Garage Setback Exception
<i>Property Address:</i>	753 Third Street East
<i>Applicant:</i>	Richard Konecky
<i>Property Owner:</i>	Richard Konecky
<i>General Plan Land Use:</i>	Low Density Residential
<i>Zoning - Base:</i>	Low Density Residential
<i>Zoning - Overlay:</i>	None

### Summary:

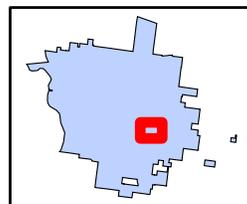
Consideration of an Exception to the garage setback requirements to enclose a carport under construction as part of a residential remodel project.



1 inch = 200 feet

## Zoning Designations

- R-HS Hillside Residential (1 D.U./10acres, maximum)
- R-R Rural Residential (2 D.U./acre, maximum)
- R-L Low Density Residential (2-5 D.U./acre)
- R-S Sonoma Residential (3-8 D.U./acre)
- R-M Medium Denisty Residential (6-10 D.U./acre)
- R-H High Density (9-12 D.U./acre)
- R-O Housing Opportunity (15-20 D.U./acre)
- R-P Mobile Home Park (7 D.U./acre, maximum)
- MX Mixed Use (12 D.U./acre, maximum)
- C Commercial (15 D.U./acre, maximum)
- C-G Commercial-Gateway (15 D.U./acre, maximum)
- W Wine Production
- P Public Facility
- Pk Park
- A Agriculture



**Item #6 – Public Hearing – Consideration of an Exception from the garage setback requirements to enclose a carport currently under construction on a residential property at 753 Third Street East.**

**Applicant/Property Owner: Richard Konecky**

Senior Planner Gjestland presented staff's report.

Comm. Howarth questioned carport requirements for the development. Staff responded that the Development Code is silent on carports.

**Chair Willers opened the item to public comment.**

Matthew McGinty, General Contractor, described the proposed change. He noted that the property owner has reached out to neighbors on the block and that they support the application.

Elizabeth Fenton, neighbor, supported the plan and is pleased with the construction project.

**Chair Willers closed the item to public comment.**

Comm. Roberson is uncomfortable with the proposal since he did not expect the home to be taken down to the foundation when it was initially approved. He considered this piecemeal planning and is not persuaded to grant the exception.

Comm. Howarth concurred with Comm. Roberson's comments.

Comm. Cribb opposed granting the Exception.

Chair Willers agreed with Comm. Cribb and expressed his opposition to the application.

Comm. Cribb made a motion to deny the application. Comm. Howarth seconded. The motion was unanimously adopted.

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**Item #7 – Public Hearing – Consideration of a Use Permit to relocate the Boys & Girls Club teen program to a commercial tenant space at 19245 Sonoma Highway.**

**Applicant/Property Owner: Boys & Girls Club of Sonoma Valley/ S & N II Ltd.**

Planning Director Goodison presented the staff report.

**Chair Willers opened the public hearing.**

Rachel Cusick, representing the applicant, explained that the Teen program has been an integral part of Sonoma Valley since 2009. She is excited to relocate from the temporary trailer to a permanent 3,000 square foot space that requires minimal tenant improvements and agreed with the revised conditions of approval in the staff report.

Michael Ross, project Architect/RDC Architects, said the shopping center location is an ideal location for the use and he agreed that bike racks could be easily provided.

Comm. Roberson recommended covered bicycle parking.

Linda Corrado, resident, is concerned with safety since the traffic flow is problematic at the shopping center driveway connection to Sonoma Highway.

Robert Berger, Berger Concrete, offered to install the bicycle racks.

Nick Haley, Teens Program Director, said the program has outgrown the existing space.

Magda, student, is excited to relocate to a more comfortable environment to do homework and participate in sports.

Jennifer, student/ten year member, considered the center her second home. She stated that more space and privacy is needed for the students.

Dusty Niles, Maxwell Village property manager, is excited to venture with the Boys and Girls club on this project.

**Chair Willers closed the public hearing.**

Comm. Roberson supported the proposal and recognized the existing traffic issues.

Comm. Howarth concurred with Comm. Roberson and supported the expansion efforts.

Comm. Felder made a motion to approve with amended conditions of approval for bicycle parking (minimum of four). Comm. Cribb seconded. The motion was unanimously approved.

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**Item #8 – Public Hearing – Consideration of a Use Permit to operate a micro-brewery in conjunction with an established restaurant use at 165 West Napa Street.**

**Applicant/Property Owner: Sherpa Hospitality, LLC/Anne Thornton**

Senior Planner Gjestland presented staff's report and noted that, since application submittal, the applicant determined that brewing activities would be better accommodated within the detached accessory building off the patio rather than in the restaurant building.

Comm. Howarth confirmed with staff that the previous restaurant at this location, Meritage, had a full liquor license.

**Chair Willers opened the public hearing.**

Ngima Sherpa, applicant/30-year restaurant owner, thanked the community and staff. He indicated that he has a restaurant in St. Helena, and this is an opportunity for a second business focusing on a different type of cuisine.

Comm. Howarth confirmed with the applicant that the property east of the building is under different ownership but under lease to provide parking for the restaurant.

Bennett Martin, resident, fully supported the proposal and said that the applicant is a first class business owner.



753 3rd Street East | Sonoma, CA 94576

Owner:  
Richard Konecky

JUL 17 2015

**RICHARD KONECKY**

1000 Chestnut Street  
Apt 4B  
San Francisco, CA 94109  
Telephone: 415-847-5303  
Email: rmkonecky@gmail.com

July 17, 2015

To: The City of Sonoma Planning Commission

RE: 753 Third Street East, Sonoma CA

I am the owner of 753 Third Street East and am writing this in connection with my Application for an exception to enclose the previously approved (and now existing) carport with a garage.

I am mindful that the plans as originally submitted provided for a carport (on what turns out to be incorrect professional advice) and that the plans were approved substantially as submitted. The house is now completed and I am making this application at the request and concensus of the neighbors and based on changed circumstances since the date the plans were first submitted.

In designing and building this house, my intent was to build a home that was in keeping with the character and scale of the neighborhood, based in part on the previously submitted Historical/Architectural Evaluation performed by Tom Origer & Associates, to listen to the concerns of my neighbors, and to add value to my home and the neighborhood. It was in addressing these concerns that I did not build to the full permitted FAR. Nor did I seek to build a second story as would have been permitted per code and which would have accommodated a full garage. The impact on the neighborhood is negligible compared to what was otherwise allowable.

Throughout the process, my neighbors have expressed their concerns about the negative visual impact of the carport. The majority of the homes on the street now have garages which tend to be neater, look better and are more secure. It should be pointed out that the proposed setback of the carport enclosure is equal to or greater than the majority of homes in the neighborhood, and that the proposed design is nearly identical in size, location, setback and style to my neighbors directly to the north. In fact, there is only one home remaining on the block with a carport which is no longer in keeping with the character and style of the rest of the neighborhood.

It had been my intent to use the property as a second home and a much needed escape from the City. While personal circumstances have changed requiring me to put the home on the market, security is still of paramount concern given the fact that my property was broken into in 2014 (see the attached police report).

While I have added security lighting controlled with motion sensors (per the Building Department requirements), the house is still open and vulnerable, certainly more so than it would be if the carport were enclosed with a garage. In addition, the lighting in the carport is a concern to the neighbors directly adjoining to the north.

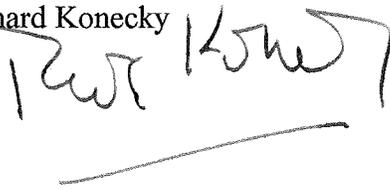
In sum, by enclosing the carport with a garage, I will not be adding to building envelope but merely enclosing what has been approved and built in keeping with the character of the neighborhood; I will be addressing the concerns of the neighbors who overwhelmingly support this application; and the threat of break-in will be significantly reduced.

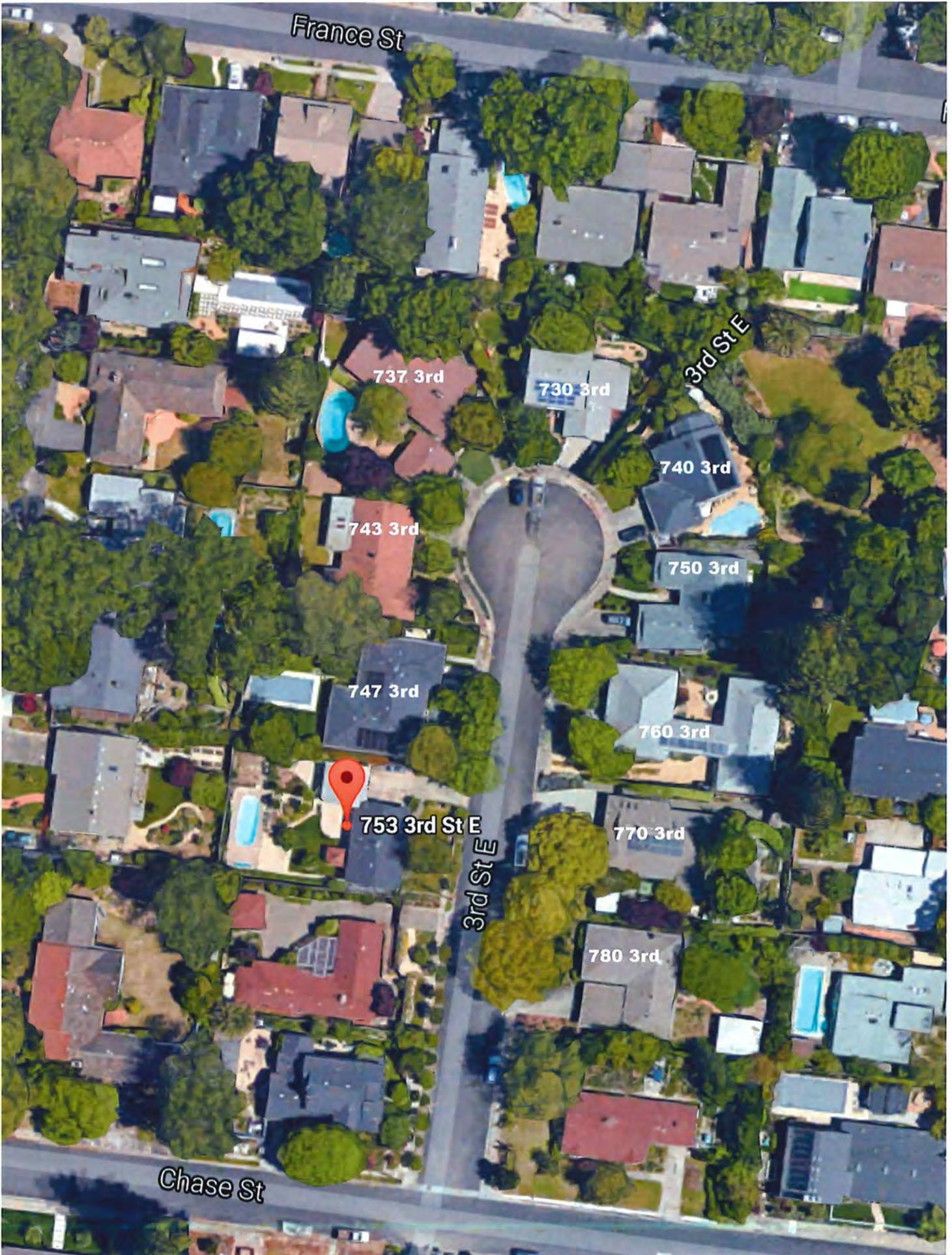
Please do not hesitate to contact me or my contractor, Matt McGinty, should you have any questions or wish to see the home. I'm certain that you will find it to be a tasteful and beneficial addition to the neighborhood. I hope you will see the proposed enclosure of the carport to to be in the best interests of the neighborhood.

Thank you.

With best regards,

Richard Konecky

A handwritten signature in black ink, appearing to read 'R. Konecky', with a horizontal line underneath it.



France St

737 3rd

730 3rd

3rd St E

740 3rd

743 3rd

750 3rd

747 3rd

760 3rd

753 3rd St E

770 3rd

3rd St E

780 3rd

Chase St



753 3rd Subject Property



747 3rd

Garage forward of house set back 3ft



743 3rd  
Garage flush with set back



737 3rd  
Garage detached and forward of house with 16ft setback



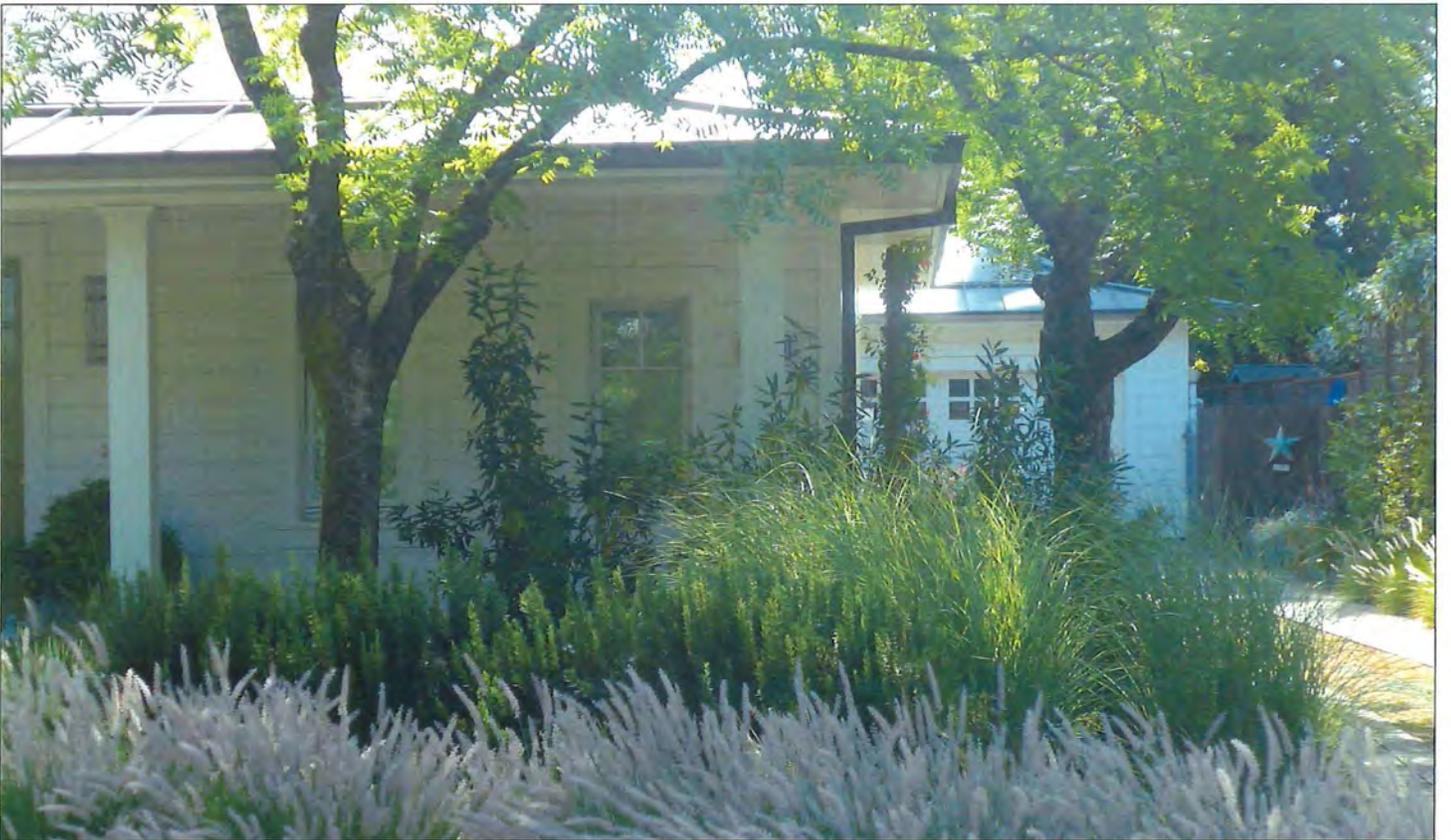
730 3rd St  
Garage 15ft forward of front of house



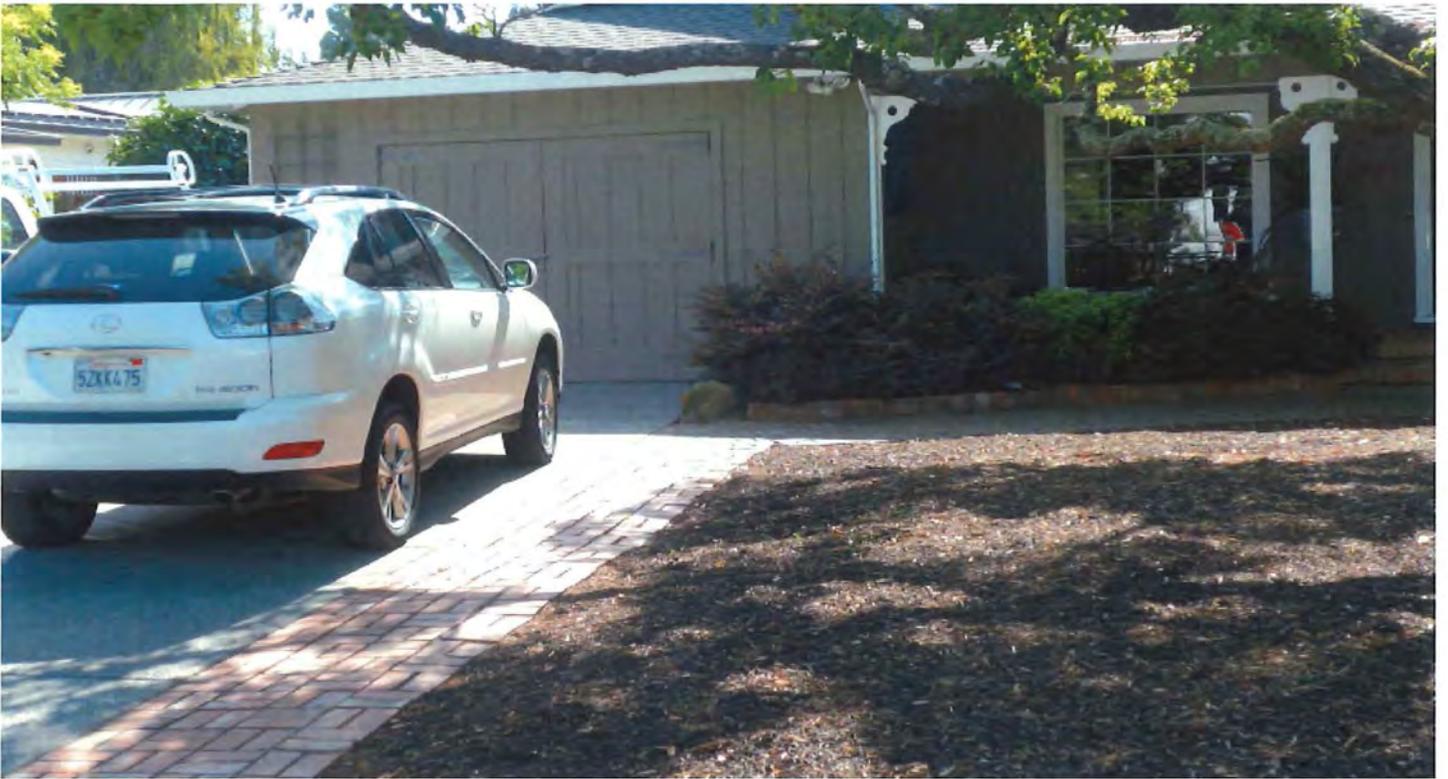
740 3rd St  
Garage forward 12ft



750 3rd st  
Garage forward 16 ft



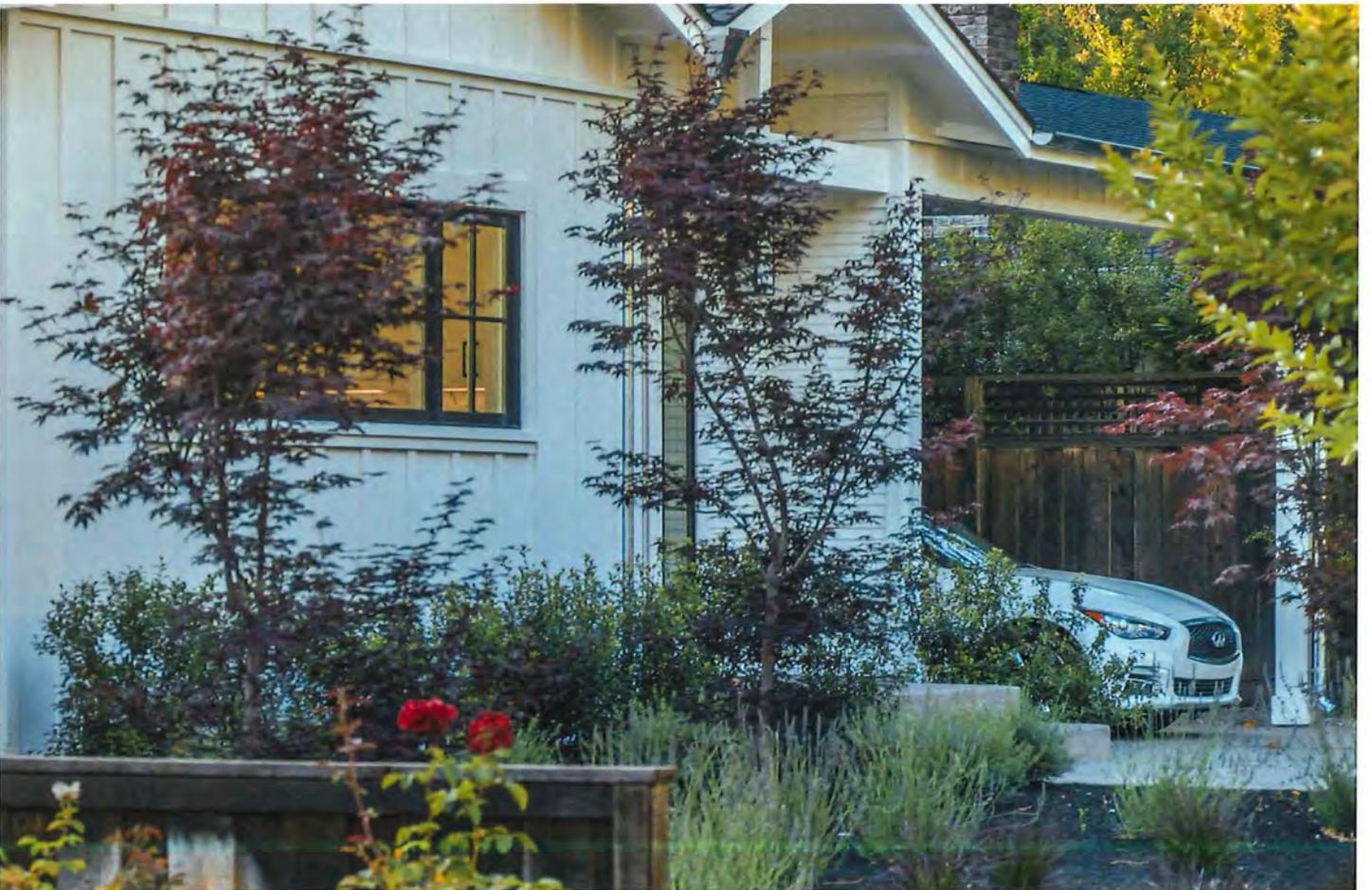
760 3rd St  
New construction garage set to rear



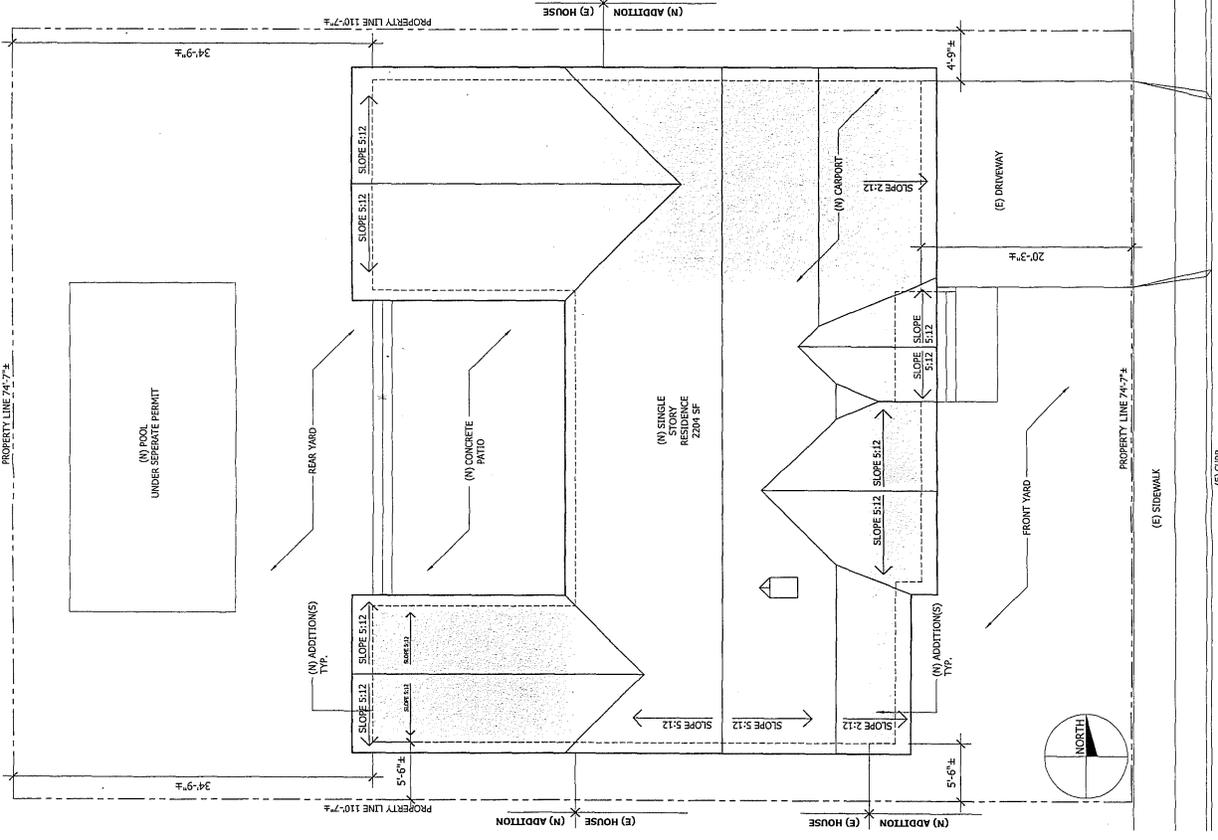
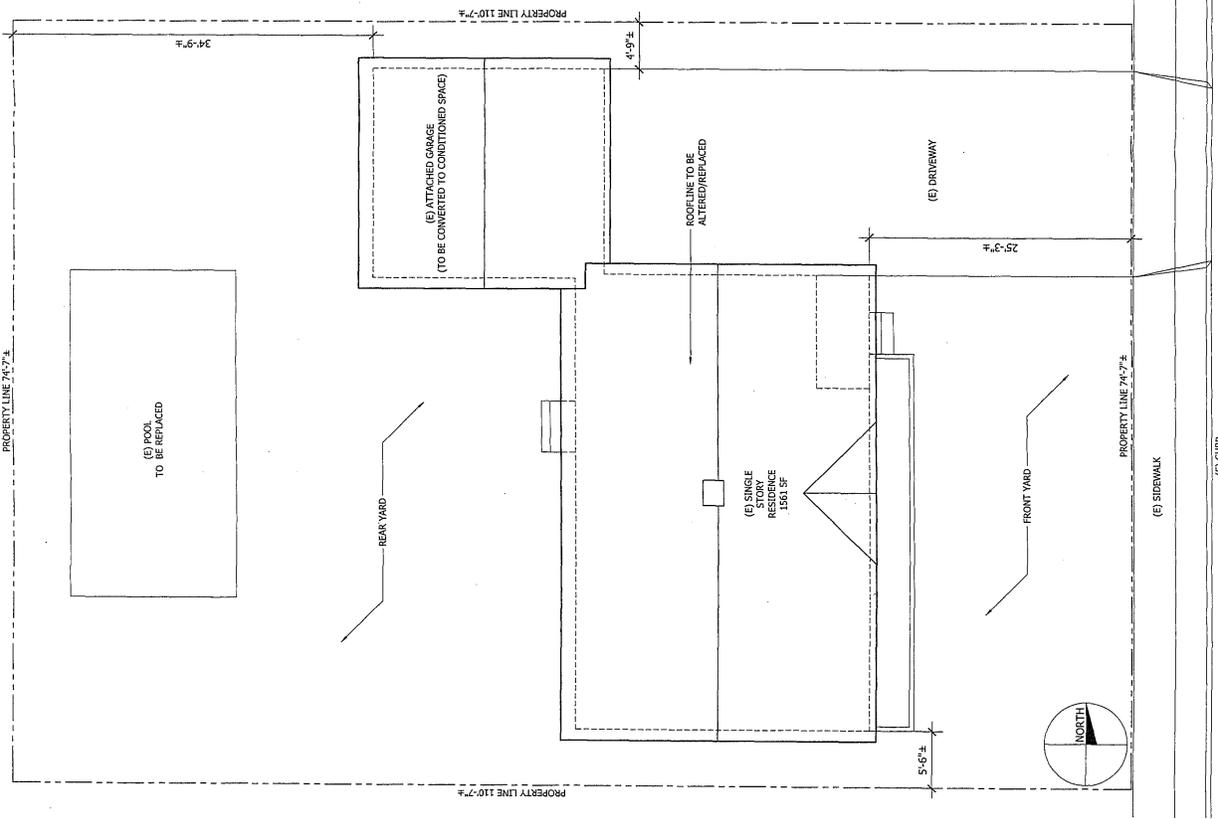
770 3rd St  
Garage flush with front of house



780 3rd St  
Original garage closed in semi-detached carport.



JUL 17 2015



DATE: 03/16/11

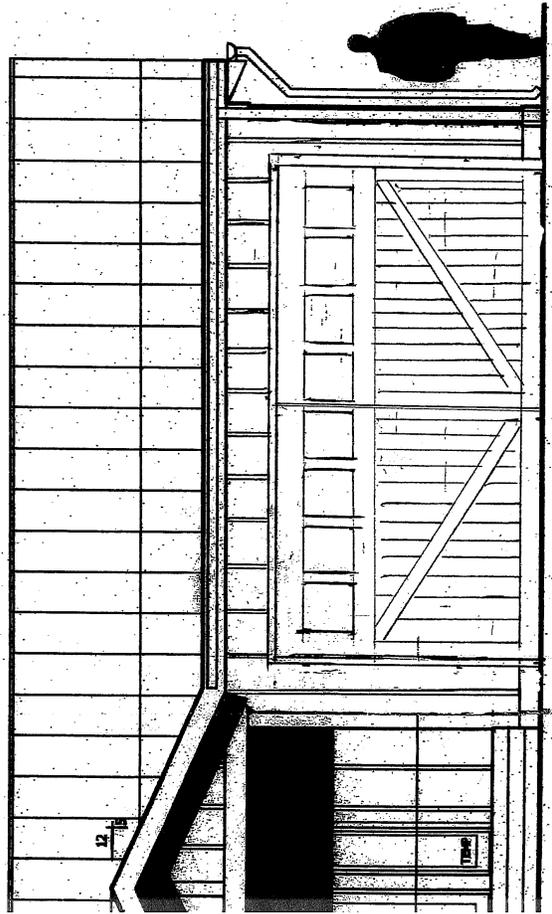
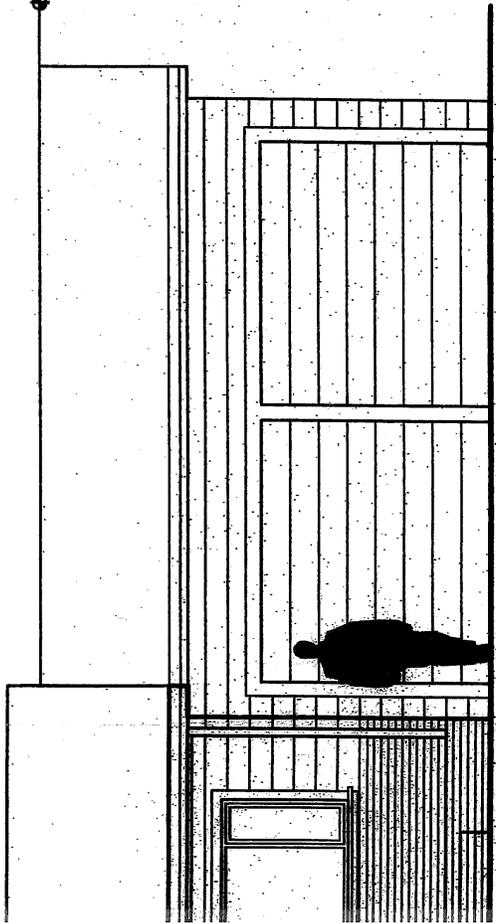
AM 730 3rd St E

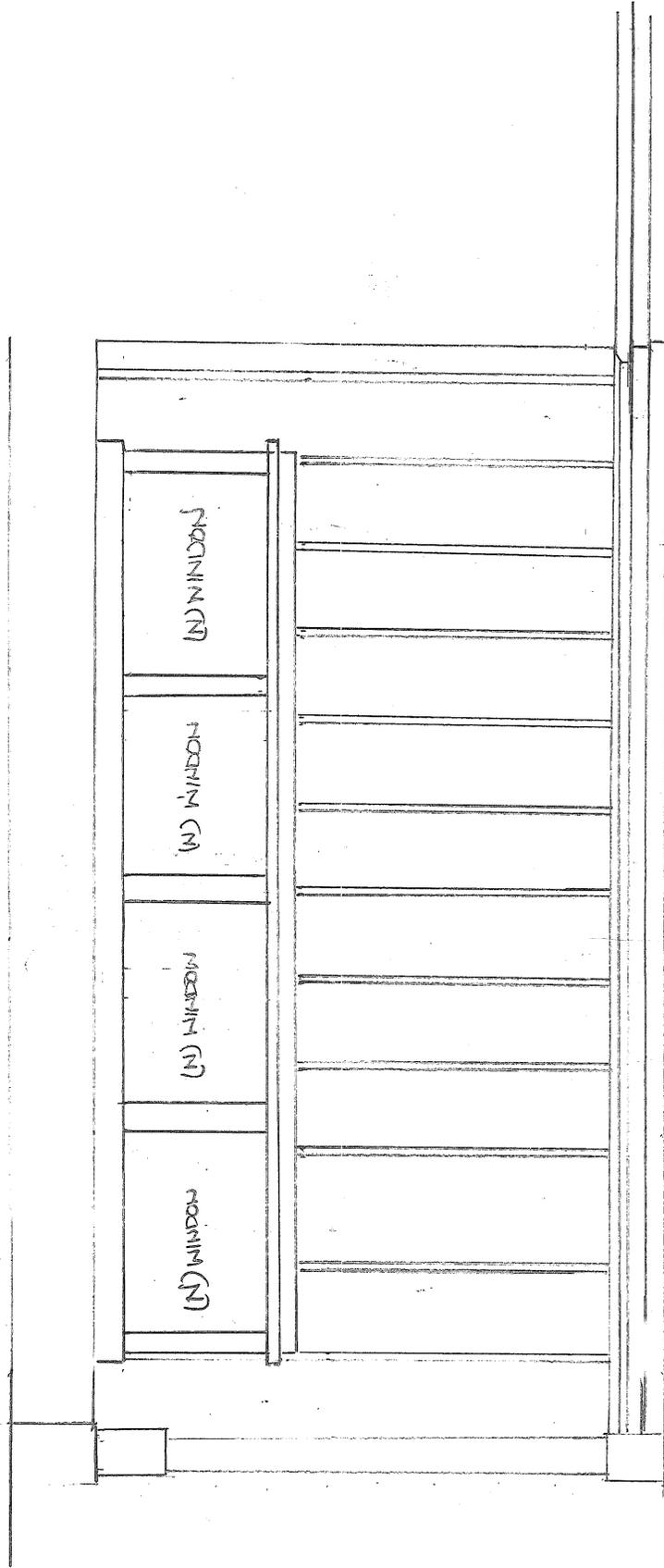
Catherine McNeill 766 Third St. East

Schumacher 770 8th St E

Vgmt 740-3655

RIDGE 12'-3 1/4"





NORTH ELEVATION  
753 3rd St East

Victor and Dale Zarzana  
740 Third Street East  
Sonoma, CA  
938.2241

Dear Sir or Madam:

This letter is to demonstrate our support for an enclosed garage at the address of 753 Third Street East, Sonoma CA

We reviewed the initial plans and found them pleasing and in conformance with our neighborhood. When asked if we'd be in favor of an enclosed garage we also indicated we would. We are now putting into writing our agreement that an enclosed garage both conforms to and supports existing structures and designs in our neighborhood.

After 60 years as east side Sonoman's (Dale's family has been on Chase Street since the 1950's) we understand the nature of what constitutes conformance in a neighborhood such as that found on the east side of Sonoma.

This design modification maintains that conformance.

We support approval.

Sincerely,

Dale and Victor Zarzana

**Subject:** 753 Third Street East

**Date:** Tuesday, August 4, 2015 at 5:43:52 PM Pacific Daylight Time

**From:** Lou & Donna Maricle

**To:** David Goodison

**CC:** matt@rs-brand.com

I am the owner of the home at 743 Third Street East, Sonoma. I understand there is a request before the planning commission to create a garage at the residence of 753 Third Street East.

I am in complete support of this construction and would encourage the planning commission to grant this request. We feel that an enclosed garage would make the home more attractive for selling purposes and would enhance the neighborhood

Thank you for your consideration.

Sincerely,

Donna Giorgi Maricle

[dlmaricle@gmail.com](mailto:dlmaricle@gmail.com)

Elizabeth M. Fenton

August, 5 2015

730 3<sup>rd</sup> Street E.

Sonoma, CA 95476

To Whom It May Concern,

My husband Andrew and I live on the cul de sac adjacent to 753 3<sup>rd</sup> Street E and have been thrilled with the progress and completion of the home's redesign and remodel. It truly is a beautiful addition to our neighborhood and of course to Sonoma.

As a close neighbor, we are privy to the homes construction and from the beginning we witnessed it exquisitely take shape. However one aspect of the build was of concern—a carport in lieu of an enclosed garage. Aesthetically the house is perfect other than this small, and easily altered, item. I'm asking the planning commission to consider allowing the construction and design team to change the exterior of the home so that it encloses the garage with a garage door. It would be more secure, more complete and will truly make this home a community gem.

We are so fortunate to live in Sonoma and have such creative talent that strive to make it more beautiful while maintaining neighborhood character, quality and tradition. I ask that you consider our request, as ultimately it is our homes, neighborhoods and communities that make Sonoma so special.

Thank you,

Elizabeth Fenton

Hi Matt,  
Thanks for sharing the plans to the house on Third St East with me tonight.  
I do not have an objection to the addition of the garage door.  
Thanks again,

--

Victor Zarzana  
740 Third St East  
Sonoma, CA 95476  
707 815-9012

Hi Matt - Looks good. We approve.

On Jan 22, 2015, at 6:44 PM, Matt McGinty <matt@rs-brand.com> wrote:

Julie,

Take a look at the rough dwg.

We aren't changing the foot print of the house, just closing side and adding 16' carriage style door.

Thanks for your help.

Matt McGinty

RS Brand

<SCAN0004.PDF>

To whom this may concern:

My residence is located south of the house being re-modeled at 753 3rd Street East. I do not object to the change in plans with regards to building a garage instead of a carport at the afore mentioned address. I think it would be more appropriate to the neighborhood to include a garage to the newly remodeled house.

Sincerely,

Karen A. Pedersen/homeowner  
767 3rd Street East  
Sonoma, CA 95476  
#707/938-1967

My name is Catherine M O'Neill and I live at 760 Third Street East, across from the above named address

I have reviewed the plans to enclose the carport and put on a garage door and I have no opposition to the plan and in fact endorse it. I view the house directly and the change/addition will only enhance the building project.

If you have any questions, please do not hesitate to call me.

Cathy O'Neill  
415/846-6552

Sent from my iPad

August 13, 2015  
Agenda Item #5

## **M E M O**

**To:** Planning Commission

**From:** David Goodison, Planning Director

**Re:** Study session on a proposal to construct a mixed-use building with ground-floor commercial and three upstairs condominiums at 19370 Sonoma Highway

### **Property Description**

The project site is composed of two adjoining parcels with a combined area of 12,654 square feet (0.29 acres). The property fronts Sonoma Highway and is bounded by Lyon Street on the north. The site is currently vacant except for a paved parking used by residents of the Sonoma Villa de Luna development located to the east. Single-family homes are located to the north and south and a vineyard is located opposite Sonoma Highway. The property has a General Plan land use designation and zoning of “Mixed Use.”

### **Background**

On July 14, 2005, the Planning Commission approved a Planned Development Permit and Use Permit for a mixed-use development at 19370 Sonoma Highway. (At that time, the development was known as “Sonoma Village West” and “Orchard Park”, but the residential component is now called “Villas de Luna”.) The approved project consisted of two commercial buildings toward Sonoma Highway with ±6,936 square feet of gross commercial floor area, eight attached town-home condominiums in the middle of the site, and seven detached single-family homes to the east. Site circulation is provided by a 28-foot wide public street off of Sonoma Highway that transitions into an 18-foot wide private road connecting to the stub of Palou Street on the east side of the site (a gate marks the transition of the public and private road section to prevent cut-through traffic). The approved project site plan is attached, along with the approved elevations of the two commercial buildings.

Construction on the residential portion of the project began in 2006. The public improvements, residential buildings, and associated landscaping were substantially complete, as was a portion of the parking lot associated with the commercial component. However, the property fell into foreclosure and construction was halted prior to final building permit sign off. In 2012, the project was acquired by the applicants, Kibby Road, LLC, which has since brought the residential portion of the project to completion. The applicants are now interested in developing the commercial portion of the site. Because the nature of the proposed development is different from what was approved in 2005, Use Permit review by the Planning Commission will be required. At this time, the applicants have requested a Planning Commission study session on the revised development plan in order to obtain feedback from the Commission prior to submitting a formal application.

## Proposed Development

The proposal involves developing the ±12,650-square foot site with a two-story, mixed use building that would include commercial use on the ground floor and three residential condominium units on the second level. The building would have a total gross floor area of 6,167 square feet, including 2,547 square feet of commercial area (two commercial suites are shown). The building also includes a shared roof deck and green roof for residents use. Parking would be provided behind the building (14 spaces, including 3 covered) with access from an existing driveway on Lyon Street. The building would be setback 18 feet from the front property line along Sonoma Highway, 11 feet from the north property line along Lyon Street, 13 feet from the side/south property line and ±50 feet from the rear (east) property line. The residential units all have roughly 1,160 square feet of living area with 2 bedrooms and 1.5 baths. A trash enclosure is proposed near the southeast corner of the building. The two parcels would be merged as part of the project. The major features of the proposed mixed-use component and the previously-approved commercial component are compared in the table below.

<b>Project Element</b>	<b>2005 Approval</b>	<b>Current Proposal</b>
Site Area	12,654 square feet	12,654 square feet
Commercial Space	6,936 square feet	2,547 square feet
Residential Units	0	3
Total Building Area	6,936 square feet	6,167 square feet
Building Height	30 feet	27 feet
FAR	0.55	0.49
Coverage	32%	20%
Parking	18 spaces	14 spaces

The proposed design of the mixed-use building departs from the approach used for that of the adjoining townhouse building and the previously approved commercial buildings. The architecture of the new building is clearly contemporary and it employs simpler forms and a flat roof. While much of the building exterior would be stucco, which relates it to the townhomes in material and color, wood siding is also used. As noted above, the originally-approved commercial component took the form of a pair of two-story buildings (see attached elevations and rendering). The revised proposal features a reduction in total building area, but takes the form of a single structure. In order to help reduce its visual mass, the building is broken down through the use of offset elements that feature contrasting materials.

## General Plan Policy Direction

The property is designated Mixed Use by the General Plan. The Mixed Use land use designation is intended to accommodate uses that provide a transition between commercial and residential districts, to promote a pedestrian presence in adjacent commercial areas, and to provide neighborhood commercial services to adjacent residential areas. The designation allows a density up to 20 residential units per acre and a residential component is required in new development. General Plan policies that apply to the project and warrant consideration by the Planning Commission include the following:

### Community Development Element

- Promote innovative design and mixed uses through the Development Code. (CDE 4.1)
- Coordinate development on small contiguous lots where possible. (CDE 4.3)
- Require pedestrian and bicycle access and amenities in all development. (CDE 4.4)
- Preserve and enhance the scale of the community without imposing rigid stylistic restrictions. (CDE 5.1)
- Promote higher density, infill development, while ensuring that building mass, scale and form are compatible with neighborhood and town character. (CDE 5.5)
- Encourage a variety of unit types in residential projects (CDE 4.2).
- Require pedestrian and bicycle access and amenities in all development (CDE 4.4).
- Promote higher density, infill development, while ensuring that building mass, scale and form are compatible with neighborhood and town character (CDE 5.5).

### *Housing Element:*

- Provide a mix of housing types affordable to all income levels, allowing those who work in Sonoma to also live in the community (HE Goal 1.0).
- Encourage diversity in the type, size, price and tenure of residential development in Sonoma, while maintaining quality of life (HE 1.1).
- Maintain and enhance the existing housing stock and ensure that new residential development is consistent with Sonoma's town character and neighborhood quality (HE Goal 3).
- Promote the use of sustainable construction techniques and environmentally sensitive design for all housing, to include best practices in water conservation, low-impact drainage, and greenhouse gas reduction (HE 6.3).

### *Environmental Resources Element:*

- Require new development to provide adequate private and, where appropriate, public open space (ERE 1.4).
- Encourage construction, building maintenance, landscaping, and transportation practices that promote energy and water conservation and reduce green-house gas emissions (ERE 3.2).

### *Circulation Element:*

- Incorporate bicycle facilities and amenities in new development (CE 2.5).
- Encourage a mixture of uses and higher densities where appropriate to improve the viability of transit and pedestrian and bicycle travel (CE 3.2).

### *Noise Element:*

- Encourage all new development to minimize noise intrusions through project design (NE 1.6).

The proposal is consistent with policies encouraging housing, including a diversity of housing types, but that being said, there are several policy areas that also need to be considered, including compatibility in terms of the building's mass, form, and setbacks at this highly visible location.

## Development Standards and Guidelines

*Use:* Commercial, multi-family residential, and mixed-use development are allowed in the Mixed Use (MX) zone, subject to review and approval by the Planning Commission.

*Consistency with Density Limitations:* The site has a General Plan land use designation and corresponding zoning of Mixed Use, which allows a maximum density of 20 residential units per acre. With the three condominiums, the project represents a density of 10 units per acre.

*Zoning Requirements:* With respect to the mixed-use zoning requirements, the building exceeds the minimum 9-foot side yard setback on the south side and minimum 25-foot rear yard setback. The project has a FAR of 0.49 and lot coverage of 20%, below the allowable levels of 0.70 and 60% respectively. The maximum building height is proposed at 27 feet, below the 30 foot height limit, and the minimum 300-square feet of open space per unit is provided in the form of private balconies and the roof deck. In terms of the Development Code, the only area of inconsistency identified to date is that the building does not meet the minimum 25-foot front and street side yard setback from the frontages on Sonoma Highway and Lyon Street. This is partly the result of a dedication of right-of-way along Highway 12 required with construction of the residential portion of the project. That said, staff would note that the proposed building setback from Sonoma Highway is greater than what was approved for the previous commercial buildings and can be considered as a modification to an approved Planned Unit Development site plan, in which reduced setbacks along Lyon Street and Sonoma Highway were specifically authorized.

*On-Site Parking:* For residential condominiums, the Development Code requires 1.5 parking spaces per unit (including one covered space), plus an additional 25% for guest parking. Accordingly, 6 spaces are required for the residential component (including 3 covered spaces). For retail, office, and personal service uses, one parking space is required per 300 square feet of floor area, which would require an additional 8 spaces for the commercial component. 14 spaces are provided on-site, including three covered spaces, which is consistent with the total parking requirement. The required 27-foot backup distance is also met. Three compact spaces having a width of eight feet are proposed, while the standard spaces have a width of nine feet.

*Design Guidelines:* Design Guidelines: In addition to quantified zoning requirements regarding setbacks, coverage, Floor Area Ratio limitations, and so forth, the Development Code sets forth design guidelines tailored to each Planning Area. Within the West Napa/Sonoma Highway Corridor, key guidelines potentially applicable to the proposed development are as follows:

- The massing of larger commercial and mixed-use buildings (5,000 square feet or greater) should be broken down to an appropriate scale through the use of storefronts and breaks in the facade.
- Architectural styles and details that reflect the Sonoma vernacular should be used.
- Site design and architectural features that contribute to pedestrian comfort and interest, such as awnings, recessed entrances, paseos, alleys, and patios, are encouraged.
- Potential impacts on adjacent residential uses shall be considered and addressed through the site planning of new commercial and mixed-use development.

## **Previous Environmental Review**

An expanded Initial Study was prepared and a Mitigated Negative Declaration adopted for the Sonoma Village West project. As evaluated in that analysis, the Sonoma Village West project included 6,936 square feet of retail/office floor area on the subject property. The current proposal scales back the total amount of building area and substitutes three condominium units for upper floor office space. With those changes, the number of vehicle trips generated by the proposed project would be reduced in comparison to the approved project (9 peak period trips rather than 13). Accordingly, the proposal does not raise any new significant environmental issues and would therefore have a somewhat lesser impact in terms of traffic generation than the previously approved project.

## **Project Issues**

*Neighborhood Compatibility:* In meetings with staff in in correspondence submitted to the Planning Commission, many neighbors in the Villa des Luna residences have voiced concern about the proposed project, including issues of compatibility. In particular, these neighbors have raised concerns in the following areas:

- **Visual Compatibility.** Neighbors are concerned that the contemporary architecture employed in the revised project is out of character with the design approach established by the townhouse building. (This issue is further discussed below.)
- **Traffic and Parking.** Neighbors are concerned that traffic generated by the project will increase waiting times at the intersection of Lyon Street and Sonoma Highway. As discussed above, the revised project would actually generate somewhat less traffic than the approved plan. With regard to parking, neighbors want assurances that on-site parking provided for the mixed-use component will be adequate, as parking in the area is limited. In this regard, the proposed development complies with on-site parking requirements. (Note: as shown on the previously-approved site plan, it was known that the public portion of Lyon Street would initially be developed without on-street parking on the north. The north parking lane, as well as curb, gutter, and sidewalk, will be developed in conjunction with the future development of the parcel at 19360 Sonoma Highway.)
- **Views.** In a meeting with staff, neighbors residing in the townhouse units noted that the proposed project would block views to the west. This would be true of the previously-approved project as well.
- **Maintenance.** The neighbors ask that the project, in whatever form it may be approved, include CC&Rs or other restrictions to ensure adequate maintenance. If condominiums are ultimately approved, they would be subject to CC&Rs or some other form of agreement that could encompass maintenance requirements.

In their letter to the Planning Commission, the neighbors also question the need for any retail or office component. This is not a compatibility issue, as such, and staff would only note that the

proposed mix of uses is allowed for in the Mixed Use zone, subject to Planning Commission approval.

*Building Mass & Design:* Although the total building area would be reduced in the revised project by approximately 11%, combining the area into a single structure may increase the appearance of building mass. As discussed above, the proposed design includes offset building elements and changes in materials to reduce the sense of mass. The contemporary nature of the proposed architecture and the flat roof makes for a different design approach than that of the adjoining townhouse building. While the use of stucco and a common base color are intended to provide some commonality, the Planning Commission needs to determine if the new design is acceptable. The flat roof does provide benefits in that it reduces the building height somewhat and allows for a roof deck that provides outdoor space for the condominium units.

*Setback on Sonoma Highway:* As noted above, the building does not meet the 25-foot front and street side yard setback from the frontages on Sonoma Highway and Lyon Street normally called for in the Development Code. In part, this results from a dedication of right-of-way along Highway 12 required in conjunction with the development of the residential component. In addition, staff would note that the proposed building location has a greater set back than that approved for the previous commercial buildings.

*Trash Enclosure:* The trash enclosure proposed on the south boundary is roughly 4' by 12' (50 square feet). The adequacy of this area to serve both the commercial and residential uses needs to be verified.

### **Next Steps**

The applicant is before the Planning Commission in a study session to obtain feedback from the Commission and receive comments from the public. Ultimately, if there is support for the project, it would return to the Planning Commission for consideration of a Use Permit/Planned Development revision and a tentative map.

### **Recommendation**

The applicants are before the Planning Commission in a study session in order to obtain feedback from the Commission and receive comments from the public at the earliest stage of the review process. Staff recommends that the Planning Commission provide direction to the applicant on the issues identified in the staff report and any other issues identified through Commission discussion or public comment.

Attachments

1. Vicinity Map
2. Correspondence
3. Project Narrative
4. Site Plan, Floor Plans, Roof Plan, Building Elevations, and Rendering
5. Previously approved Site Plan, Elevations, and Rendering

cc: Alicia Hansel (via email)  
Kibby Road, LLC  
2269 Chestnut St. #242  
San Francisco CA 94123

Sonoma Villas De Luna email list

# Vicinity Map

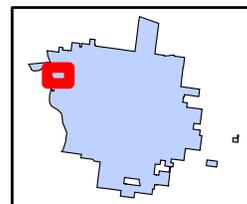


## Zoning Designations

- R-HS Hillside Residential (1 D.U./10acres, maximum)
- R-R Rural Residential (2 D.U./acre, maximum)
- R-L Low Density Residential (2-5 D.U./acre)
- R-S Sonoma Residential (3-8 D.U./acre)
- R-M Medium Denisty Residential (6-10 D.U./acre)
- R-H High Density (9-12 D.U./acre)
- R-O Housing Opportunity (15-20 D.U./acre)
- R-P Mobile Home Park (7 D.U./acre, maximum)
- MX Mixed Use (12 D.U./acre, maximum)
- C Commercial (15 D.U./acre, maximum)
- C-G Commercial-Gateway (15 D.U./acre, maximum)
- W Wine Production
- P Public Facility
- Pk Park
- A Agriculture

### Project Summary

<i>Project Name:</i>	Kibby Homes Study Session
<i>Property Address:</i>	19366-19370 Sonoma Highway
<i>Applicant:</i>	Kibby Homes
<i>Property Owner:</i>	Kibby Road LLC
<i>General Plan Land Use:</i>	Mixed Use
<i>Zoning - Base:</i>	Mixed Use
<i>Zoning - Overlay:</i>	N/A
<i>Summary:</i>	Study session on a proposal to construct a mixed-use building with ground floor commercial use and three upstairs condominiums.



1 inch = 200 feet

Talibah Chiku  
875 Lyon Street, #31, Bldg. 6  
Sonoma, CA 95476

RECEIVED

July 27, 2015

JUL 27 2015

Planning Commission  
Sonoma City Hall, No. 1  
The Plaza  
Sonoma, CA 95476

CITY OF SONOMA

Re: Application of G&C Auto Body

→ Mixed-Use Building, 19366-19370 Sonoma, Highway (Kibby Homes)

If one of these projects is on the corner of Lyon and Sonoma Highway, please ensure some traffic controls are placed near/on Lyon Street from which 50+cars come and go daily from Lyon street. The traffic is such that residents sometimes wait 10-15 minutes or more before safely entering Sonoma Highway to proceed either left or right while attempting to take a lane to reach Napa or Railroad! The traffic is non-stop, especially doing some peak morning, afternoon and evening traffic.

Any project of the scope, size and commercial use is a potential detriment to residents entering the Highway from Lyon Street with traffic no control. Safety is essential. Please do not overlook this need. Those living behind the gate further down Lyon come out through Lyon Street because they can or they exit down their "private street" which is off-limits from we living in the Valley Oak apartments off Lyon!

I'm moving and still, this issue demands consideration. Thank you.

Ms. Talibah Chiku

August 6, 2015

Planning Commission  
No.1, The Plaza  
Sonoma, California 95476

RE: Mixed-Use Development Proposal for 19366 and 9370 Sonoma Highway

Dear Commissioners:

The homeowners of Sonoma Villas de Luna are happy to participate as citizens in the democratic planning process and to have our voices heard.

We are grateful to own homes in Sonoma, one of the most livable cities in California. Like anyone with a treasure, we want to protect our homes and our way of life. Lots 19366 and 19370 became mixed-use in 1995—20 years ago.

Because the area has developed gradually, and not with a specific use plan, we are concerned that the passage of time has changed the conditions.

Here are our questions::

**1. What is the evidence that additional retail space is necessary at this location?**

Although the property is mixed-use, we know that it can be developed as condominiums or apartments, without retail space. The City needs residential housing, but does it need retail space? Maxwell Village Shopping Center, for example, is less than one-quarter mile away, and each day 30,000 cars drive by it. Is the location of the mixed-use lots a good one to handle this much commercial traffic? Does the City need 2,547 additional square feet of retail space?

**2. What is the evidence that additional professional office space is needed in Sonoma?**

The developer indicates that she has been approached about offering some professional office space. While there may be some Sonomans who need office space, how much of a demand is there for this? Is the

developer obligated to try to satisfy the needs of some of the City's citizens unless their needs are substantial?

**3. Has the passage of time altered the original parking plan?**

When the townhomes were originally approved, it was contemplated that the street parking would be available to them. Since then, some of the street has become a no-parking zone. In addition, 43 affordable units have been built. The residents of the affordable housing units regularly park on the street. Even if the Lyon Street entrance to the affordable apartments were closed off to cars, people would still park on the street and walk in, as they do now.

Three condominiums would require 5.162 parking spaces.  
3,000 square feet of retail space would require 10 spaces.  
The 8 townhomes require three guest parking spaces.

Is there enough parking?

**4. Two different pictorial versions of the building have been submitted. One is compatible with the townhomes, one does not seem to be. Which version is the correct one?**

**5. If the building is not a part of the SVL HOA, would it have its own HOA so that the residents of our community could be assured of the following:**

- a. Appropriate landscape maintenance
- b. Appropriate painting and repairs

Thank you for your time and attention to addressing our vital concerns. We are not concerned about increasing our wealth; we are concerned about our families and our way of life.

Very sincerely yours,

The Homeowners of Sonoma Villas de Luna:  
Jack Ding, Nick Dolata; Joan Jennings; Maria Pecavar; Brian Rowlands

July 21, 2015

## 19370 SONOMA HIGHWAY - MIXED-USE DEVELOPMENT PROPOSAL

Assessor's Parcel Number: 127-760-001 & 002

Project Sponsor: Alicia Hansel, Kibby Road LLC

Architect: Studio 101 Designs

### PROJECT NARRATIVE

The proposal includes the construction of a 6,167 SF, two-story, mixed-use building. Occupancy includes commercial use on the ground level, three residential condominium units on the second level, and a shared roof deck and green roof for residential use.

Residential open space is achieved through a combination of private balconies, accessed through residences, and a shared roof deck. Commercial open space is achieved through hardscape and planted landscape set within the front yard setback.

Construction will be a combination of factory-built modules for residential units and site-built elements for the ground level, vertical circulation, roof elements and siding.

Landscaping will include engineered bioretention facilities in the front and side yards to meet the City's stormwater management requirements.

### EXTERIOR FINISHES:

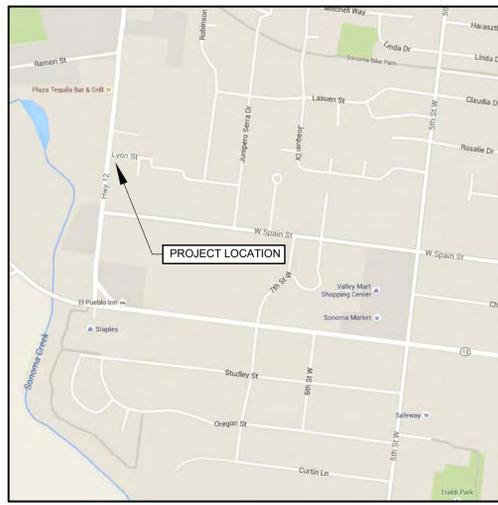
1. Stucco siding with smooth trowel finish - colors closely matched to existing townhouses behind development
2. Cedar tongue-and-groove siding
3. Dark bronze anodized aluminum doors and windows
4. Metal railings and decks, finished to match windows

### SITE DEVELOPMENT SUMMARY:

1. Combined lot area: 12,654 sf (0.29 acres)
2. Proposed building footprint: 2,547 sf (20% lot coverage)
3. Proposed gross floor area: 6,167 sf (0.49 far)
4. Residential open space: 1,066 sf (900 required)
5. Commercial open space: 1,476 sf (11% of site), front yard and center walkway
6. Ground floor area: 2,547 sf
7. Second floor area: 3,620 sf
8. Roof deck area: 705 sf
9. Private decks: 361 sf

**ABBREVIATIONS**

ADJ.	Adjacent
A.F.F.	Above Finish Floor
ALUM.	Aluminum
ANOD.	Anodized
ATT.	Attenuation
@	At
BD.	Board
BETW.	Between
BLKG.	Blocking
BM.	Beam
B.O.	Bottom Of / Blockout
BSMT.	Basement
B.U.R.	Built-Up Roof
C.B.B.	Cement Backer Bd.
CEM.	Cement / Cementitious
C.I.P.	Cast In Place
CIV.	Civil
C.J.	Control Joint
CL.	Center Line
CLNG.	Ceiling
CLR.	Clear
C.M.U.	Concrete Masonry Unit
COL.	Column
CONC.	Concrete
CONST.	Construction
CONT.	Continuous
DBL.	Double
D.G.	Double Glazed
DIM.	Dimension
DN.	Down
DR.	Door
DTL.	Detail
DWG.	Drawing
EA.	Each
EL.	Elevation
E.L.J.	Expansion Joint
ELEC.	Electrical
ELEV.	Elevator
EQ.	Equal
EQUIP.	Equipment
EXP.	Expansion
EXT.	Exterior
(E)	Existing
F.C.	Fiber Cement
F.E.C.	Fire Extinguisher Cabinet
F.F.L.	Finish Floor Level
FLR.	Floor
FLUOR.	Fluorescent
FIN.	Finish
FNDN.	Foundation
F.O.	Face of Stud
F.O.S.	Face of Wall
F.O.W.	Furring
FURR.	Foundation Vent
F.V.	Foundation Vent
GA.	Gauge
G.S.M.	Galvanized Sheet Metal
GALV.	Galvanized
GLAZ.	Glazing
GR.	Grade
G.W.B.	Gypsum Wall Board
GYP. BD.	Gypsum Board
H.C.	Hollow Core
H.D.	Hot Dipped
H.M.	Hollow Metal
HR.	Hour
HT.	Height
INS.	Insulation
INT.	Interior
JT.	Joint
LEV.	Level
LT.	Light
LOC.	Location
M.U.	Masonry Unit
MAX.	Maximum
MECH.	Mechanical
MEMB.	Member
MFR.	Manufacturer
MIN.	Minimum
MTD.	Mounted
MTL.	Metal
MOD.	Module
N.I.C.	Not In Contract
NO.	Number
(N)	New
O/A	Overall
O.C.	On Center
OPNG.	Opening
OPP.	Opposite
PRE-FIN.	Pre Finished
PL.	Property Line
PLAS.	Plaster
PLAS. LAM.	Plastic Laminate
PLT.	Plate
PLY.	Plywood
PT.	Point
PTD.	Painted
RAD.	Radius/Radii
R.W.L.	Rain Water Leader
R.D.	Roof Drain
RWD.	Redwood
RE.	Refer to
REFR.	Refrigerator
RES.	Resistant
RESIL.	Resilient
REQD.	Required
RGD.	Rigid
RM.	Room
R.O.	Rough Opening
R.O.D.	Rolling Overhead Door
S.C.	Solid Core
SCHED.	Schedule
SECT.	Section
SHT.	Sheet
SIM.	Similar
SKD. GD.	Skid Guard
ST. STL.	Stainless Steel
STD.	Stained
STRUCT.	Structural
SUSP.	Suspended
THK.	Thick
THRU.	Through
T.O.	Top of
T.O.C.	Top of Concrete
T.O./STL.	Top of Steel
T.O.W.	Top of Wall
TRANSF.	Transformer
T.S.	Tube Steel
TYP.	Typical
U.O.N.	Unless Otherwise Noted
U/S	Underside
VEN.	Veneer
VEST.	Vestibule
VER.	Verify
V.I.F.	Verify In Field
w/	With
WD.	Wood
W.P.	Water Proofing
W.R.	Water Resistant
WT.	Weight



VICINITY MAP

NOT TO SCALE

# MIXED-USE DEVELOPMENT

19370 SONOMA HIGHWAY  
SONOMA, CA 95476  
APN: 127-760-001 & 002

## GENERAL NOTES

- ALL INFORMATION SHALL COMPLY WITH THE 2010 CALIFORNIA BUILDING, PLUMBING, MECHANICAL, AND ELECTRICAL CODES, AND ALL OTHER APPLICABLE NATIONAL, STATE, AND LOCAL CODES, RULES AND REGULATIONS HAVING JURISDICTION.
- DO NOT SCALE DRAWINGS FOR DIMENSIONS.
- ALL DIMENSIONS ARE TO FACE OF STUDS UNLESS OTHERWISE NOTED.
- INSULATION SHALL MEET CALIFORNIA ENERGY COMMISSION QUALITY STANDARDS AND BE CERTIFIED BY THE MANUFACTURER.
- ALL WINDOWS AND DOORS SHALL BE DOUBLE GLAZED U.O.N.
- ALL GLAZING IN DOORS, WITHIN 24" OF DOORS AND ADJACENT TO BATHTUBS AND SHOWERS, SHALL BE TEMPERED PER CBC.
- GENERAL LIGHTING IN KITCHENS AND BATHS SHALL HAVE AN EFFICIENCY OF 40 LUMENS PER WATT.
- ALL TOILETS ARE TO BE ULTRA LOW FLOW 1.6 GALLONS MAXIMUM FLUSH CAPACITY.
- SHOWERS: WALLS TO BE NONABSORBENT TO MIN. 72" ABOVE DRAIN. FINISH FLOOR SLOPE TO BE 1/4" TO 1/2" PER FT. PAN LINER TO ROLL OVER TOP OF ROUGH THRESHOLD CURB AND FASTEN TO OUTSIDE EDGE; WHERE NO CURB, PAN LINER TO LAP UNDER ADJACENT FLOOR BACKER BOARD MIN. 1'-0". WEEP HOLES REQUIRED AT DRAIN; WEEP HOLES TO REMAIN CLEAR AND UNOBSTRUCTED BY MORTAR.
- SMOKE DETECTORS SHALL BE INTERCONNECTED AND "HARD" WIRED IN CEILINGS NEAR ALL SLEEPING AREAS AS PER CBC 314.
- ALL WATER PIPING TO BE COPPER PIPE.
- ALL DRAINAGE PIPING TO BE ABS SCHEDULE 40.
- SUBSTITUTIONS: FACTORY AND CONTRACTOR TO SUBMIT ALL PROPOSED SUBSTITUTIONS TO ARCHITECT FOR APPROVAL. ACCEPTABLE SUBSTITUTIONS SHALL BE EQUIVALENT TO SPECIFIED MATERIAL OR PRODUCT.

## CONTACTS

OWNER: ALICIA HANSEL  
KIBBY ROAD LLC  
415-215-8356  
ALICIA@KIBBYROAD.COM

ARCHITECT / OWNER AGENT: SCOTT LANDRY  
LEVI CONOVER  
STUDIO 101 DESIGNS  
101 H STREET, SUITE C  
PETALUMA, CA 94952  
707-778-0101  
SCOTT@STUDIO101DESIGNS.COM  
LEVI@STUDIO101DESIGNS.COM

## SCOPE OF WORK

THE PROPOSAL INCLUDES THE CONSTRUCTION OF A 6,167 SF, TWO-STORY, MIXED-USE BUILDING. OCCUPANCY INCLUDES COMMERCIAL USE ON THE GROUND LEVEL, THREE RESIDENTIAL CONDOMINIUM UNITS ON THE SECOND LEVEL, AND A SHARED ROOF DECK AND GREEN ROOF FOR RESIDENTIAL USE.

RESIDENTIAL OPEN SPACE IS ACHIEVED THROUGH A COMBINATION OF PRIVATE BALCONIES, ACCESSED THROUGH RESIDENCES, AND A SHARED ROOF DECK. COMMERCIAL OPEN SPACE IS ACHIEVED THROUGH HARDSCAPE AND PLANTED LANDSCAPE SET WITHIN THE FRONT YARD SETBACK.

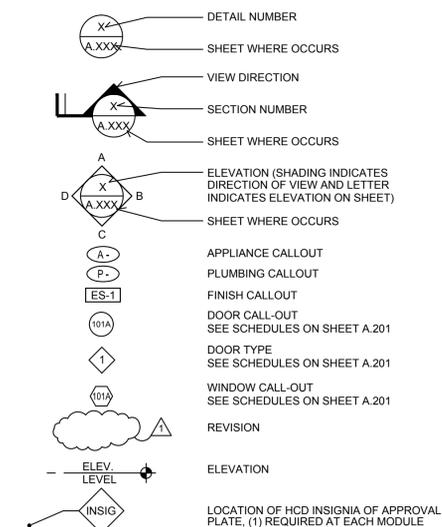
CONSTRUCTION WILL BE A COMBINATION OF FACTORY-BUILT MODULES FOR RESIDENTIAL UNITS AND SITE-BUILT ELEMENTS FOR THE GROUND LEVEL, VERTICAL CIRCULATION, ROOF ELEMENTS AND SIDING.

LANDSCAPING WILL INCLUDE ENGINEERED BIORETENTION FACILITIES IN THE FRONT AND SIDE YARDS TO MEET THE CITY'S STORMWATER MANAGEMENT REQUIREMENTS.

EXTERIOR FINISHES:

- STUCCO SIDING WITH SMOOTH TROWEL FINISH - COLORS CLOSELY MATCHED TO EXISTING TOWNHOUSES BEHIND DEVELOPMENT
- CEDAR TONGUE-AND-GROOVE SIDING
- DARK BRONZE ANODIZED ALUMINUM DOORS AND WINDOWS
- METAL RAILINGS AND DECKS, FINISHED TO MATCH WINDOWS

## DRAWING SYMBOLS



## CODE ANALYSIS

PARCEL NUMBER: A.P.N. 127-760-001 & 002

COMBINED LOT AREA: 12,654 SF

BASE ZONING: C

COMBINING DISTRICT: NONE

CONSTRUCTION TYPE: B (1ST FLR), R-2 (SECOND FLR)

OCCUPANCY GROUP: COMMERCIAL & MULTI-FAMILY

GROSS FLOOR AREA: 6,167 SQ. FT.

RESIDENTIAL FLOOR AREA: 3,620 SQ. FT.

COMMERCIAL FLOOR AREA: 2,547 SQ. FT.

SPRINKLERED: Y

BUILDINGS DESIGNED TO MEET THE FOLLOWING CODES:

BUILDING CODE: 2013 CBC  
RESIDENTIAL CODE: 2013 CRC  
MECHANICAL CODE: 2013 CMC  
ELECTRICAL CODE: 2013 CEC  
PLUMBING CODE: 2013 CPC  
FIRE CODE: 2013 CFC  
ENERGY CODE: 2013 CALIFORNIA ENERGY CODE (CCR T24, PART 6)  
GREEN BUILDING CODE: 2013 CALIFORNIA GREEN BUILDING STANDARDS CODE

FACTORY-BUILT MODULES DESIGNED TO MEET THE FOLLOWING CODES:

CALIFORNIA CODE OF REGULATIONS, TITLE 25, DIV 1, CH 3, SUBCH 1  
CALIFORNIA HEALTH AND SAFETY CODE, DIV 13, PART 6

## SHEET INDEX

T.001 COVER SHEET - VICINITY MAP, GENERAL INFORMATION, SHEET INDEX

ARCHITECTURAL

A.101 EXISTING SITE PLAN  
A.102 PROPOSED SITE PLAN  
A.211 LOWER FLOOR COMMERCIAL SPACE PLAN  
A.221 UPPER FLOOR RESIDENTIAL UNITS  
A.231 ROOF PLAN  
A.301 EXTERIOR ELEVATIONS  
A.302 EXTERIOR ELEVATIONS

studio 101 designs  
101 H St., SUITE C, Petaluma, CA 94952  
ph. 707.778.0101  
www.studio101designs.com

NOT FOR CONSTRUCTION

SONOMA MIXED-USE DEVELOPMENT  
19370 SONOMA HWY, SONOMA CA (APN: 127-760-001 & 002)

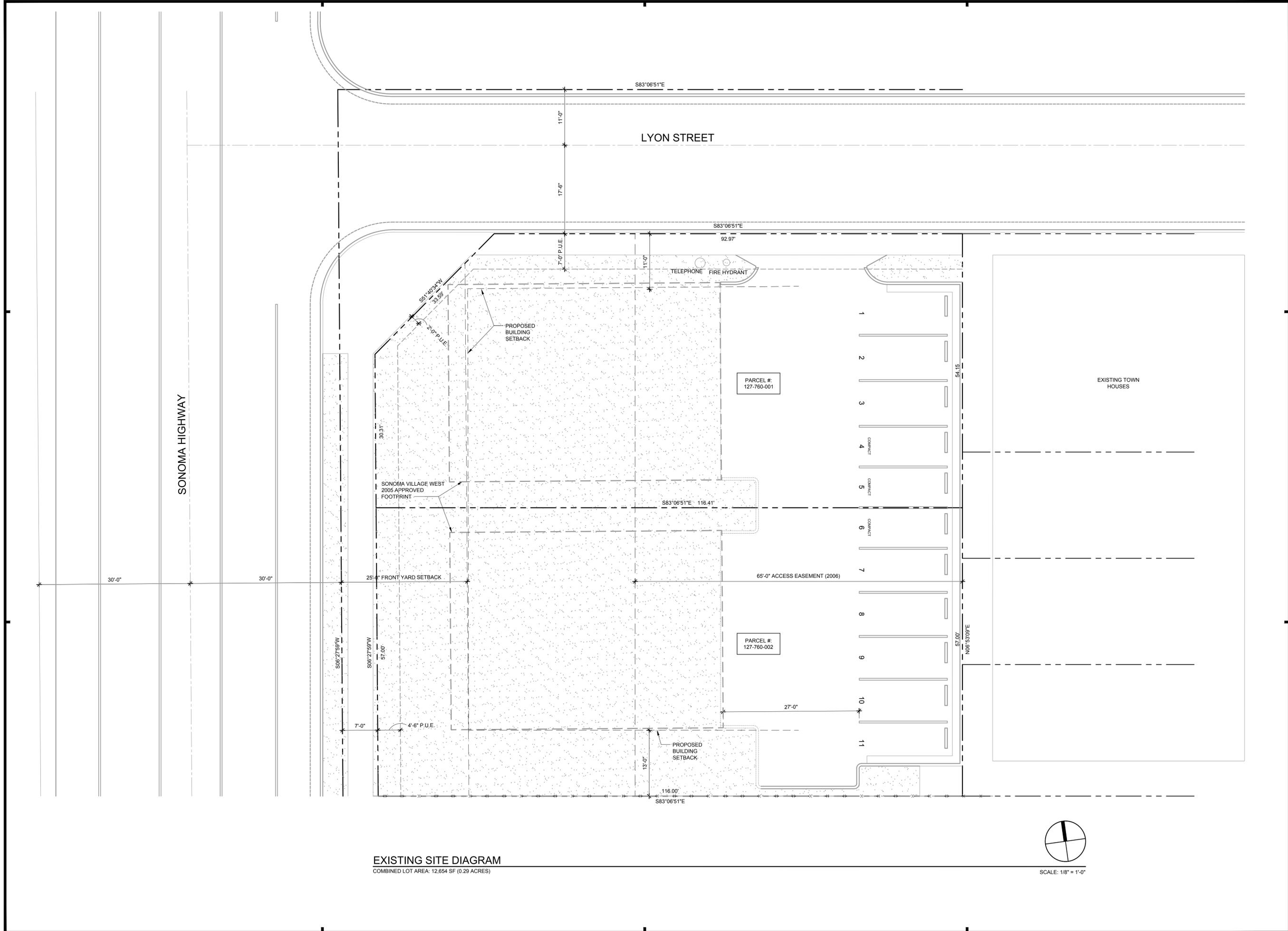
COVER SHEET

Rev	By	Description	Date
	LC	PLING. COMMISSION	7/29/15

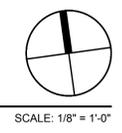
Designer:	SL	LC
Drawn By:		
Date:	05/18/15	
Project No.:	201503	
Scale:	AS SHOWN	

Sheet:

T.001



**EXISTING SITE DIAGRAM**  
 COMBINED LOT AREA: 12,654 SF (0.29 ACRES)



**NOT FOR CONSTRUCTION**

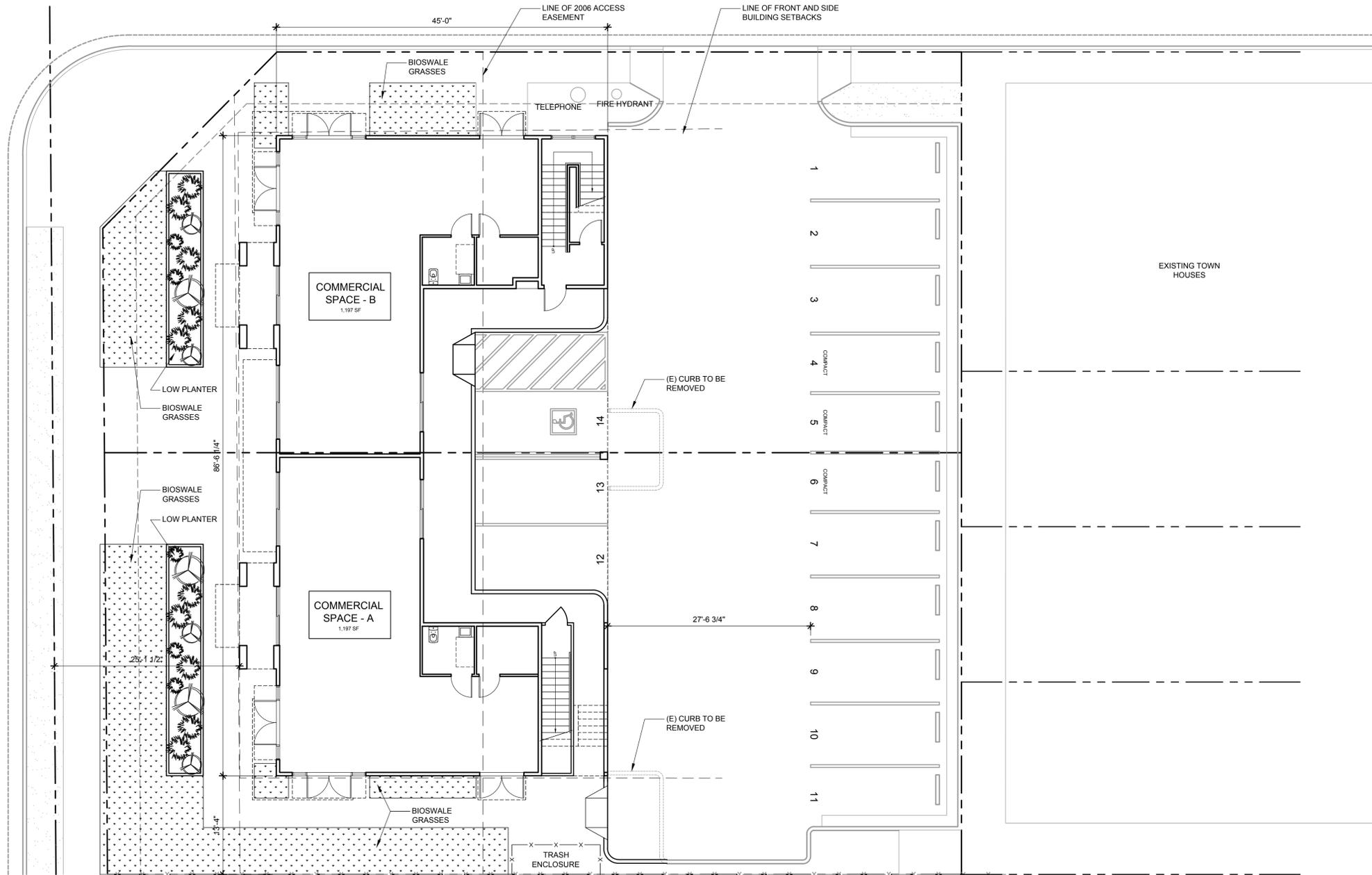
**SONOMA MIXED-USE DEVELOPMENT**  
 19370 SONOMA HWY, SONOMA CA (APN: 127-760-001 & 002)  
**EXISTING SITE DIAGRAM**

Rev	By	Description	Date
	LC	PLNG. COMMISSION	7/29/15

Designer:	SL	LC
Drawn By:		
Date:	05/18/15	
Project No.:	201503	
Scale:	AS SHOWN	

SONOMA HIGHWAY

LYON STREET



**PROPOSED SITE PLAN**

COMBINED LOT AREA: 12,654 SF (0.29 ACRES)  
 PROPOSED BUILDING FOOTPRINT: 2,394 SF (20% LOT COVERAGE)  
 PROPOSED GROSS FLOOR AREA: 6,167 SF (0.49 FAR)  
 RESIDENTIAL OPEN SPACE: 1,066 SF (900 REQUIRED)  
 COMMERCIAL OPEN SPACE: 1,476 SF (11% OF SITE), FRONT YARD AND CETER WALKWAY



SCALE: 1/8" = 1'-0"

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 www.studio101designs.com

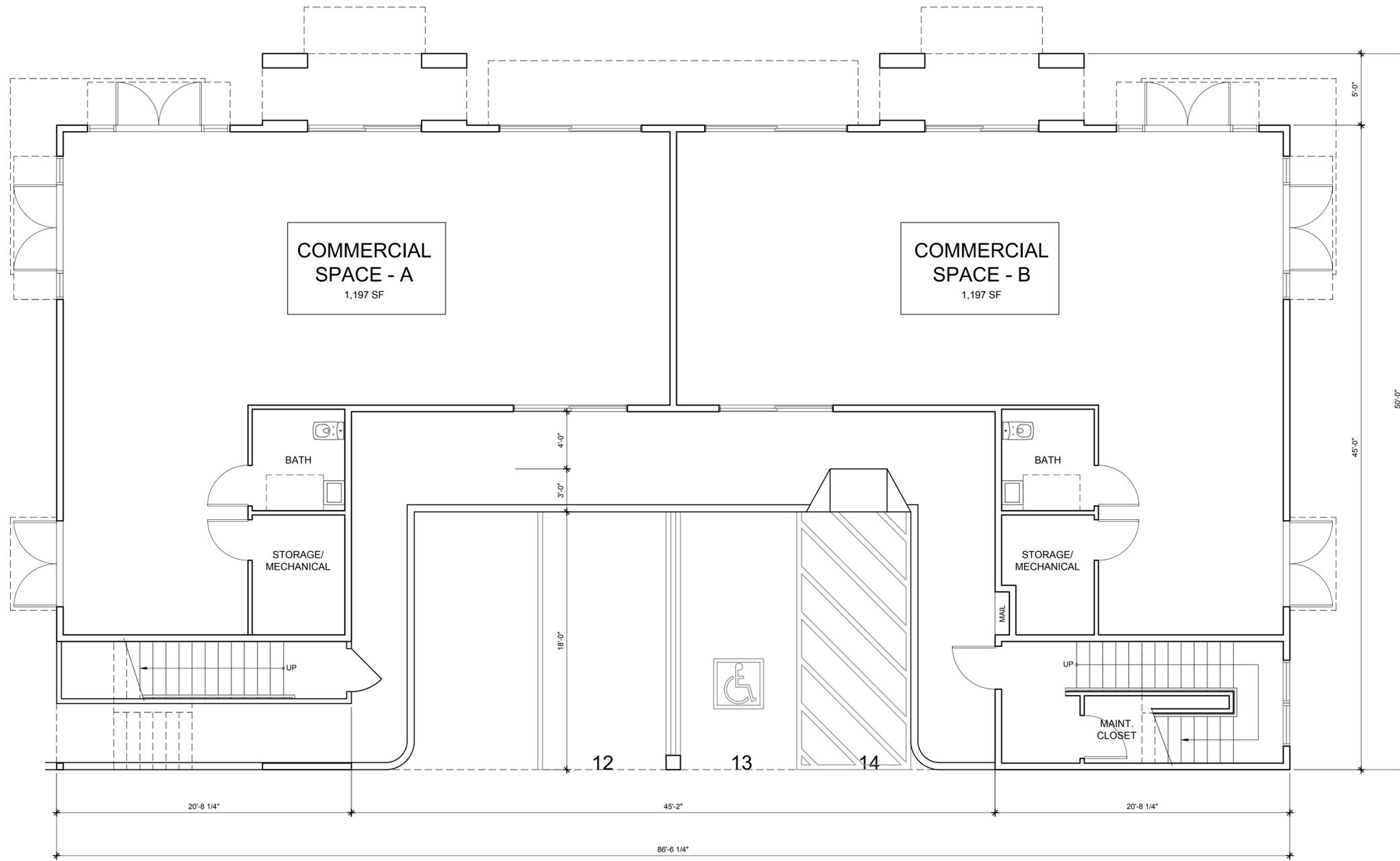
NOT FOR CONSTRUCTION

Project: SONOMA MIXED-USE DEVELOPMENT  
 19370 SONOMA HIWY, SONOMA CA (APN: 127-760-001 & 002)  
 Title: PROPOSED SITE PLAN

Rev	By	Description	Date
	LC	PLING COMMISSION	7/29/15

Designer:	SL	LC
Drawn By:		
Date:	05/18/15	
Project No.:	201503	
Scale:	AS SHOWN	

Sheet: A.102



LOWER FLOOR PLAN - COMMERCIAL SPACE  
GROUND FLOOR AREA: 2,547 SF



SCALE: 1/4" = 1'-0"

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ph. 707.778.0101  
www.studio101designs.com

NOT FOR CONSTRUCTION

Project: SONOMA MIXED-USE DEVELOPMENT  
19370 SONOMA HIWY, SONOMA CA (APN: 127-760-001 & 002)  
Title: LOWER FLOOR COMMERCIAL SPACE PLAN

Rev	By	Description	Date
	LC	PLING COMMISSION	7/29/15

Designer:	SL	LC
Drawn By:		
Date:	05/18/15	201503
Project No.:		AS SHOWN
Scale:		

Sheet: A.211



UPPER FLOOR PLAN - RESIDENTIAL UNITS  
 GROUND FLOOR AREA: 3,620 SF



SCALE: 1/4" = 1'-0"

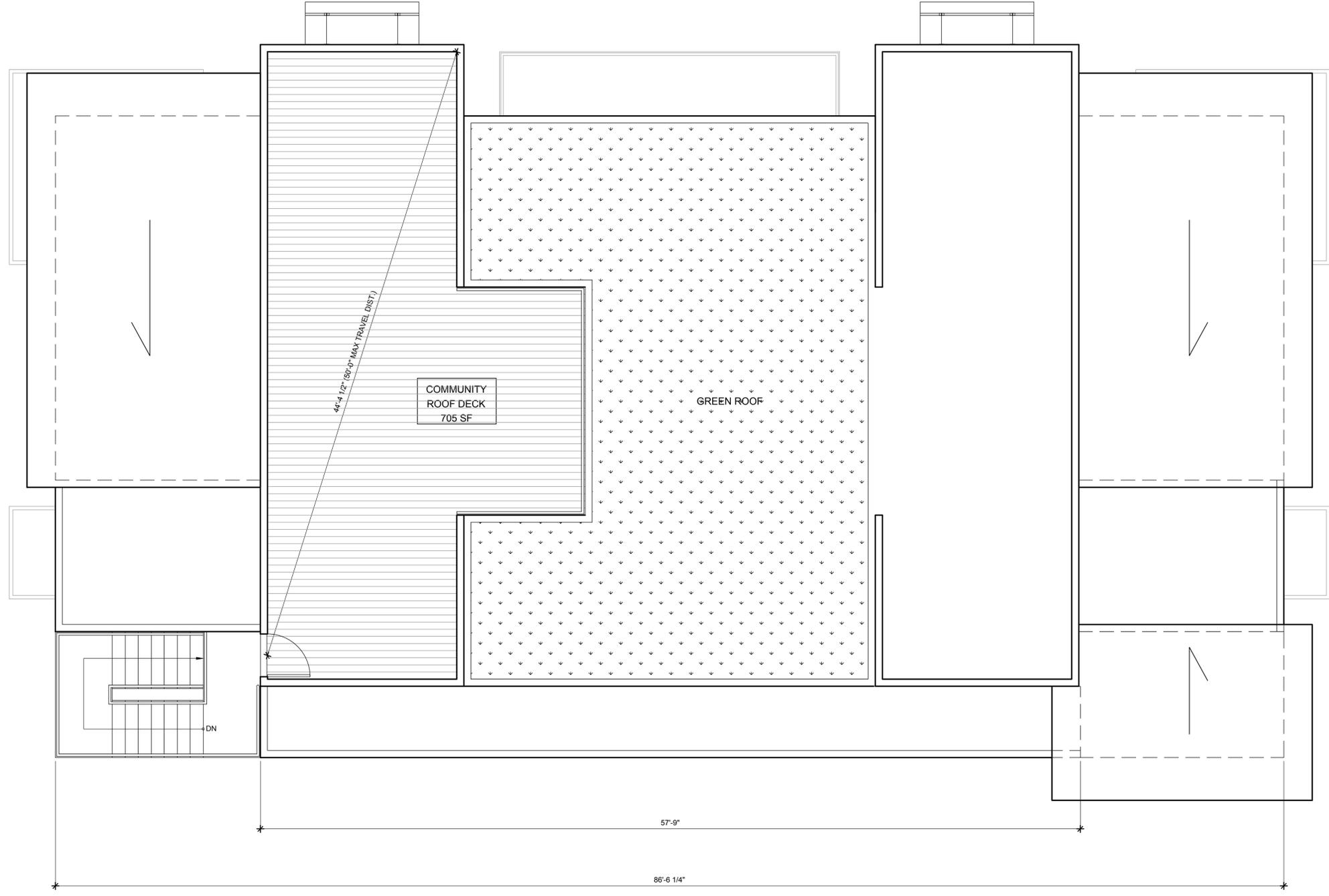
NOT FOR CONSTRUCTION

Project: SONOMA MIXED-USE DEVELOPMENT  
 19370 SONOMA HWY, SONOMA CA (APN: 127-760-001 & 002)  
 Title: UPPER FLOOR RESIDENTIAL UNITS

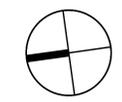
Rev	By	Description	Date
	LC	PLNG. COMMISSION	7/29/15

Designer:	SL
Drawn By:	LC
Date:	05/18/15
Project No.:	201503
Scale:	AS SHOWN

Sheet: A.221



**ROOF PLAN**  
 ROOF DECK AREA: 705 SF



SCALE: 1/4" = 1'-0"

Rev	By	Description	Date
	LC	PLING COMMISSION	7/29/15

Designer:	SL
Drawn By:	LC
Date:	05/18/15
Project No.:	201503
Scale:	AS SHOWN

Sheet:  
**A.231**

Project:  
**SONOMA MIXED-USE DEVELOPMENT**  
 19370 SONOMA HWY, SONOMA CA (APN: 127-760-001 & 002)

Title:  
**ROOF PLAN**

**NOT FOR CONSTRUCTION**

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 101 H St., SUITE C, Petaluma, CA 94952  
 ph. 707.778.0101  
 www.studio101designs.com

All drawings and written material appearing herein constitute original and unpublished work of the architect and may not be duplicated, used or disclosed without the written consent of the architect.

PARAPET ELEV.  
EL. 27.00'

ROOF DECK ELEV.  
EL. 23.50'

2ND FINISH FLOOR ELEV.  
EL. 13.50'

1ST FINISH FLOOR ELEV.  
EL. 0.00'



EXTERIOR BUILDING ELEVATION - SOUTH FACADE  
LYON STREET

SCALE: 1/4" = 1'-0"

2

PARAPET ELEV.  
EL. 27.00'

ROOF DECK ELEV.  
EL. 23.50'

2ND FINISH FLOOR ELEV.  
EL. 13.50'

1ST FINISH FLOOR ELEV.  
EL. 0.00'



EXTERIOR BUILDING ELEVATION - EAST FACADE  
SONOMA HIGHWAY

SCALE: 1/4" = 1'-0"

1

studio 101 designs

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NOT FOR  
CONSTRUCTION

SONOMA MIXED-USE DEVELOPMENT  
19370 SONOMA HWY, SONOMA CA (APN: 127-760-001 & 002)

EXTERIOR ELEVATIONS

Project:

Title:

Rev	By	Description	Date
	LC	PLNG. COMMISSION	7/29/15

Designer:	SL	LC
Drawn By:		
Date:	05/18/15	
Project No.:	201503	
Scale:	AS SHOWN	

Sheet:

A.301



EXTERIOR BUILDING ELEVATION - NORTH FACADE  
SIDE PROPERTY LINE

SCALE: 1/4" = 1'-0" **2**



EXTERIOR BUILDING ELEVATION - WEST FACADE  
REAR PARKING LOT

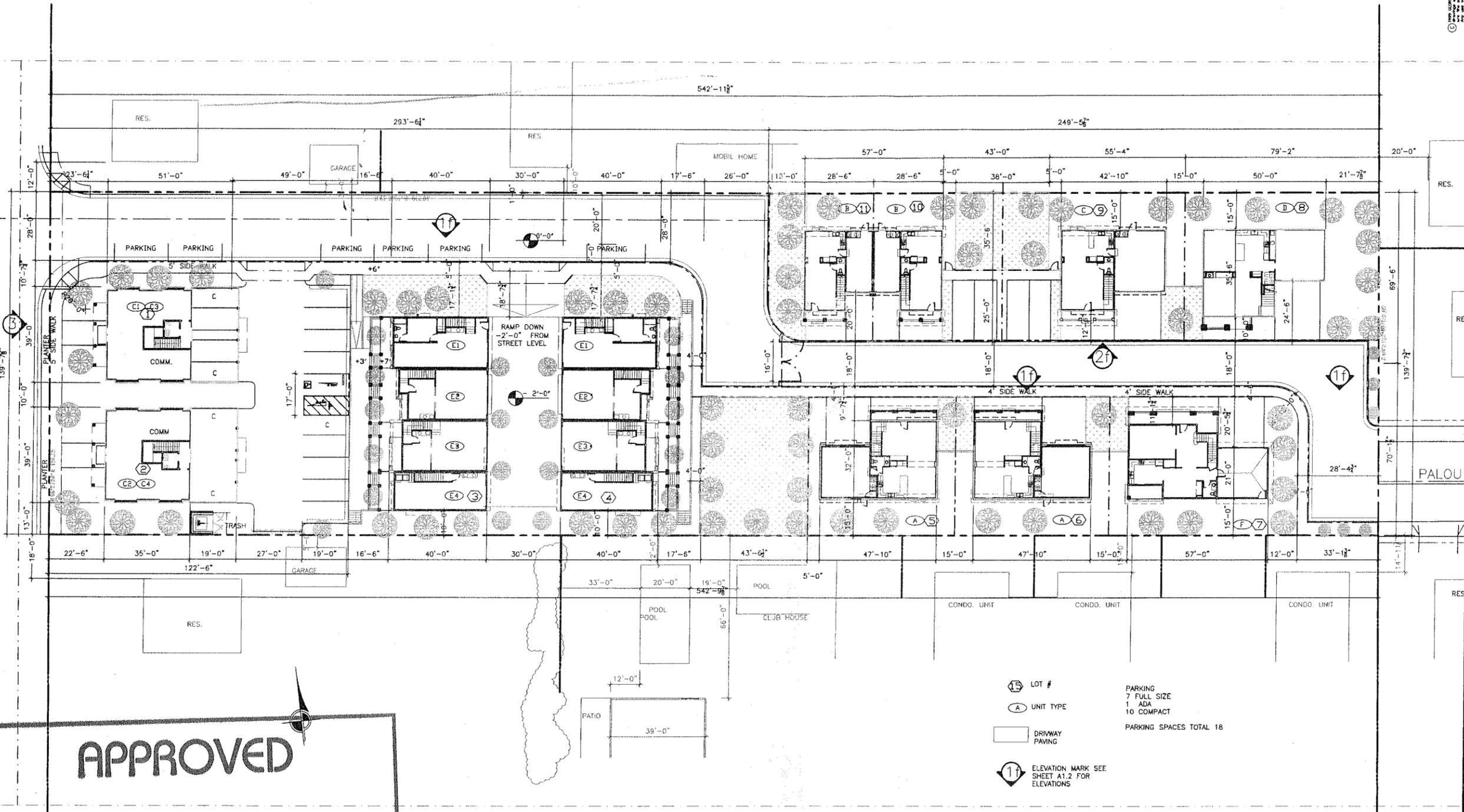
SCALE: 1/4" = 1'-0" **1**

Rev	By	Description	Date
	LC	PLING COMMISSION	7/29/15

Designer:	SL	LC
Drawn By:		
Date:	05/18/15	
Project No.:	201503	
Scale:	AS SHOWN	



CALIFORNIA HIGHWAY 12



**APPROVED**

By: DRC

Date: 9/20/05

Comments: \_\_\_\_\_

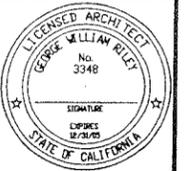
SITE PLAN

1"=20'-0"

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PROPOSED SITE IMPROVEMENT  
FOR  
**SONOMA VILLAGE WEST**  
APN# 127-202-11  
19370 SONOMA HIGHWAY, SONOMA CALIFORNIA



**GEORGE WILLEY & ASSOCIATES, INC.**  
ARCHITECTS & PLANNERS  
652 PETALUMA AVE. SEBASTOPOL, CA.  
PHONE: 707.823.8767 FAX: 707.823.8712

JOB NO.	0413
DRAWN BY	Zuhoor sh.
CHKD BY	
SCALE	AS SHOWN
PRINT DATE	08/12/05
ISSUE DATE	

DRAWING NO.  
**A1.0**  
SHT 3 OF 17

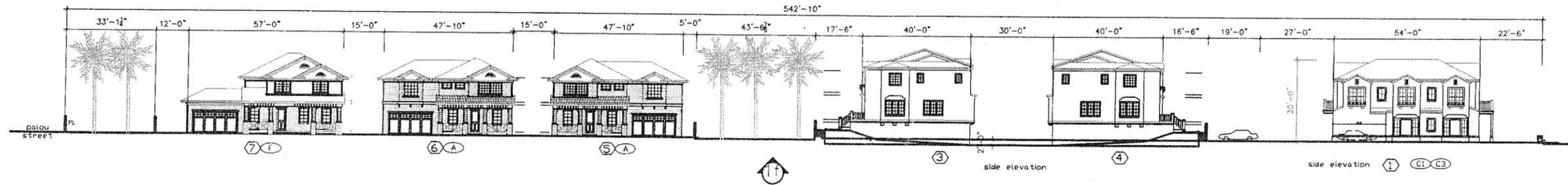


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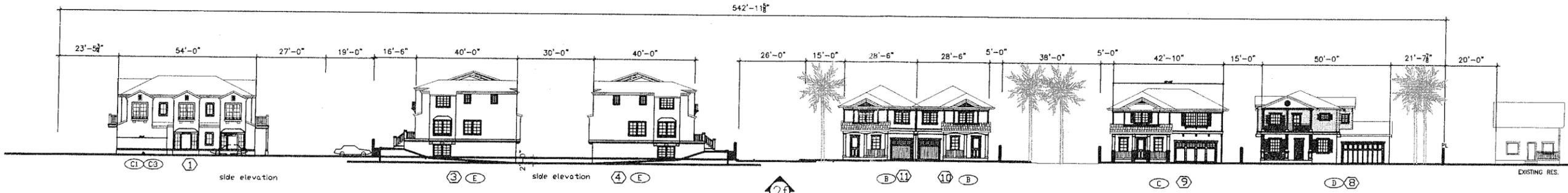
FRONT ELEVATION (WEST)

SCALE = 1/8" = 1'-0"



FRONT ELEVATION (SOUTH)

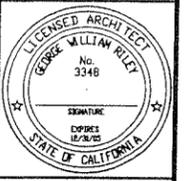
SCALE = 1" = 20'-0"



FRONT ELEVATION (NORTH)

SCALE = 1" = 20'-0"

PROPOSED SITE IMPROVEMENT  
FOR  
**SONOMA VILLAGE WEST**  
APN# 127-202-11  
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PRINT DATE	08/12/05
ISSUE DATE	

DRAWING NO.  
**A1.2**  
SHT 3 OF 17



SIDE ELEVATION COMMERCIAL BUILDING

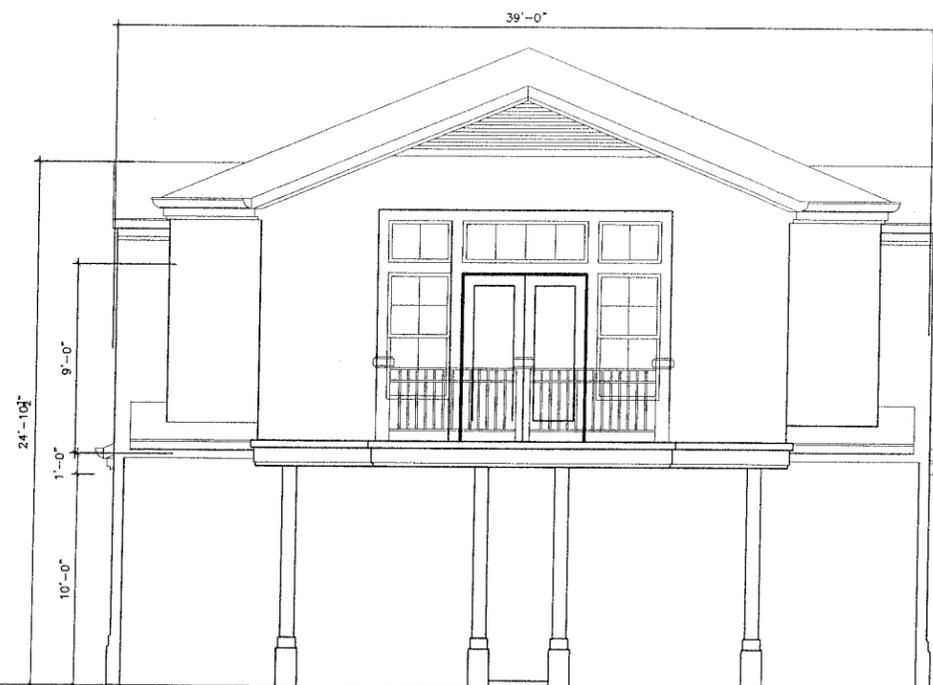


FRONT ELEVATION COMMERCIAL BUILDING

SCALE 1/4" = 1'-0"



SIDE ELEVATION COMMERCIAL BUILDING



REAR ELEVATION COMMERCIAL BUILDING

SCALE 1/4" = 1'-0"

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PROPOSED SITE IMPROVEMENT  
FOR  
**SONOMA VILLAGE WEST**  
APN# 127-202-11  
19370 SONOMA HIGHWAY, SONOMA CALIFORNIA



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DRAWN BY	
CHKD BY	
SCALE	AS SHOWN
PRINT DATE	08/12/08
ISSUE DATE	

DRAWING NO.  
**A5.0**  
SHT 11 OF 17



*Commercial*

AUG 30 2005

**M E M O**

**To:** Planning Commission  
**From:** Planning Director Goodison  
**Re:** Consideration of an amendment to the Development Code identifying “Prescribed Grazing” as a conditionally-allowed use on the “Park” zone

---

**Background**

The Montini Preserve encompasses approximately 98 acres of open space lands, including a significant portion of Sonoma’s hillside backdrop, located immediately north of the Vallejo Home State Park, extending from Fifth Street West to First Street West. The Preserve encompasses rolling grasslands, oak woodland, and a 9-acre pasture. The Sonoma County Agricultural Preservation and Open Space District (SCAPOD) acquired the Montini Preserve (and a conservation easement on an adjoining parcel) from the Montini family in 2005, with financial assistance from the California State Coastal Conservancy and the City of Sonoma. Ownership of the Preserve was transferred to the City of Sonoma in 2014.

The parcels comprising the Preserve, along with the adjoining property on the west, which is still owned by the Montini family, have been grazed for many years. Grazing is not only a historic agricultural use of the Preserve, it also has benefits in terms of vegetation management and fire protection. In the acquisition of the Preserve, it was anticipated that grazing would continue and this activity is specifically authorized in the approved Management Plan for the Preserve. However, grazing is not recognized as an allowed use in the “Park” zone, which means that it is a prohibited activity.

Because the City would like the option to allow continued grazing of the Montini Preserve, staff has developed a draft ordinance that would identify “Prescribed Grazing” as a conditionally-allowed use in the “Park” zone. “Prescribed Grazing” refers to the practice of grazing as a means of vegetation management. This activity, as defined, would not conflict with the prohibition on dairies, stockyards, and animal farms established by Section 8.08.020 of the Municipal Code.

**Environmental Review**

The allowance for continued grazing was evaluated in the Management Plan for the Montini Preserve and the accompanying initial study and negative declaration. No additional environmental review is required.

**Recommendation**

Staff recommends that the Planning Commission hold a public hearing on the proposed amendments to the Development Code and recommend to the City Council that they be adopted.

Attachments:

1. Draft Ordinance

# CITY OF SONOMA

## ORDINANCE NO. X - 2015

### AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF SONOMA AMENDING TITLE 19 OF THE SONOMA MUNICIPAL CODE BY ESTABLISHING ‘PRESCRIBED GRAZING’ AS A CONDITIONALLY-ALLOWED USE IN THE “PARK” ZONE

The City Council of the City of Sonoma does ordain as follows:

**Section 1.** Amendments to “Zones and Allowable Uses” (Title 19, Section 19.10.050) of the Sonoma Municipal Code.

Table 2-4 (Special Purpose Uses and Permit Requirements) is hereby amended as follows:

<i>Allowed Uses and Permit Requirements for Special Purpose Zoning Districts</i>	<i>Permit Required by District</i>				<i>Specific Use Regulations</i>
	<i>A</i>	<i>Pk</i>	<i>P</i>	<i>W</i>	
<i>Land Use (1)</i>	<i>A</i>	<i>Pk</i>	<i>P</i>	<i>W</i>	<i>Specific Use Regulations</i>
<i>Agricultural and Open Space Uses</i>					
<i>Crop Production and Horticulture</i>	<i>P</i>	<i>—</i>	<i>—</i>	<i>P</i>	
<i>Livestock Raising</i>	<i>P</i>	<i>—</i>	<i>—</i>	<i>—</i>	
<i>Prescribed Grazing</i>	<i>—</i>	<i>UP</i>	<i>—</i>	<i>—</i>	<i>19.50.020</i>
<i>Produce Stands for On-site Production</i>	<i>P</i>	<i>—</i>	<i>—</i>	<i>—</i>	
<i>Trails, Hiking, and Bicycling</i>	<i>P</i>	<i>P</i>	<i>P</i>	<i>—</i>	<i>19.50.070</i>
<i>Notes:</i>					
1. See Section 19.10.050.C regarding uses not listed. See Division VIII for definitions of the listed land uses.					
2. New residential developments subject to the City’s Growth Management Ordinance (SMC 19.94).					
3. Supportive and transitional housing shall be subject to those restrictions that apply to other residential dwellings of the same type in the same zone.					

**Section 2.** Amendments to “Definitions” (Title 19, Division VIII) of the Sonoma Municipal Code.

Section 19.92.020 (Definitions of Specialized Terms and Phrases) is hereby amended as follows:

“Prescribed Grazing: The application of livestock (cattle, sheep or goats) to feed on standing forage as a landscape management technique to control invasive plant species and reduce wildland fire hazards in a manner that preserves desirable natural characteristics.”

**Section 3.** Exemption from Environmental Review.

The amendments to the Municipal Code effected by this ordinance are exempt from environmental review pursuant to Section (b)(3) of title 14 of the California Code of Regulations, as it can be determined with certainty that there is no possibility that the proposed revisions to the Development Code, which are intended to implement directions set forth in the Housing Element and comply with State law, will not have any significant impact on the environment.

**Section 4.** Effective Date.

This ordinance shall become effective thirty (30) days from and after the date of its passage.

PASSED, APPROVED AND ADOPTED by the City Council of the City of Sonoma this XX day of XX, 2015.